



Heritage Assessment


Randwick Junction Town Centre Planning Strategy

Randwick NSW 2031

Submitted to Randwick City Council

JUNE 2023

REPORT REVISION HISTORY

Revision	Date Issued	Revision Description		
01	08/02/2023	DRAFT		
		Prepared by	Reviewed by	Verified by
		Asmita Bhasin <i>Heritage Consultant</i> Kerime Danis <i>Director - Heritage</i>	Kerime Danis <i>Director - Heritage</i>	 Kerime Danis <i>Director - Heritage</i>
02	10/05/2023	DRAFT 2 (Upon Feedback from Randwick City Council)		
		Prepared by	Reviewed by	Verified by
		Asmita Bhasin <i>Heritage Consultant</i>	-	-
03	16/05/2023	FINAL		
		Prepared by	Reviewed by	Verified by
		Asmita Bhasin <i>Heritage Consultant</i> Kerime Danis <i>Director - Heritage</i>	Kerime Danis <i>Director - Heritage</i>	Kerime Danis <i>Director - Heritage</i>
04	06/06/2023	FINAL 2		
		Prepared by	Reviewed by	Verified by
		Asmita Bhasin <i>Heritage Consultant</i> Kerime Danis <i>Director - Heritage</i>	Kerime Danis <i>Director - Heritage</i>	Kerime Danis <i>Director - Heritage</i>

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We proudly operate from the lands of the Gadigal, Darkinyung, Danggari Balun and Turrbal Peoples.

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City Plan Heritage P/L

Level 6, 120 Sussex St, Sydney NSW 2000
 P +61 2 8270 3500

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TABLE OF CONTENTS

1. Introduction	5
1.1. Background.....	5
1.2. Context of the Planning Strategy.....	5
1.3. The Site	5
1.4. Heritage Listing.....	6
1.5. Methodology	9
1.6. Documentation	9
1.7. Limitations.....	9
1.8. Author Identification.....	10
2. Site Context and Description	11
2.1. Site Context	11
2.2. Site Description	13
3. Historical Context.....	17
3.1. Aboriginal History	17
3.2. Brief History of Randwick	17
3.3. Development of Randwick Junction Heritage Conservation Area.....	18
3.4. Heritage Items	27
3.5. Heritage Conservation Areas	55
3.6. Potential Heritage Items	57
4. Significance of Randwick Junction Town Centre.....	61
5. The Proposed Planning Strategy.....	63
5.1. Three Key Development Scenarios.....	65
5.1.1. Mid-rise mixed-use buildings in strategic sites	65
5.1.2. New Infill Development in Valued Heritage Context.....	65
5.1.3. Incremental modifications - heritage/contributory buildings.....	65
6. Opportunities and Constraints	67
6.1. Constraints.....	67
6.2. Opportunities	68
7. Analysis of Vision For Randwick Junction Town Centre.....	70
8. Conclusion and Recommendations	93
Appendix A.....	96
The Proposed Planning Strategy	96

FIGURES

Figure 1: Study Area (indicated in blue) for Randwick Junction Town Centre	6
Figure 2: Heritage Map for the subject study area within its surrounding context.	8
Figure 3: Prince of Wales Hospital complex located opposite subject site along High Street and Avoca Street,	11
Figure 4: Randwick Light Rail station along High Street	12
Figure 5: Low-to-medium density development at the corner of Alison Road and Botany Street	12
Figure 6: Multistorey flat buildings along High Street, opposite Randwick Light Rail Station	13
Figure 7: Wales Medical Centre along High Street	14
Figure 8: Junction of Avoca Street, High Street and Belmore Road in Randwick	14
Figure 9: Commercial development along Belmore Road	15
Figure 10: Multi-storey early development along Belmore Road	15
Figure 11: Corner of Alison Road and Belmore Road.....	16
Figure 12: Multi-storey mixed-use development along Belmore Road	16
Figure 13: c.1880's drawing by H. Solomon shows the Avoca Estate, Randwick	19
Figure 14: watercolour by A. Fenton Spencers depicts the corner of Alison Road and Avoca Street c1895.	20
Figure 15: Captain Cook Statue at corner of Avoca Street and Belmore Road in 1883	20
Figure 16: Avoca Street looking South c.1900.	21
Figure 17: The eastern side of Belmore Road, Randwick c.1925.....	21
Figure 18: 1930 Aerial of Randwick Junction	22
Figure 19: 1943 Aerial of the subject site showing an increase in development along Belmore Road	23
Figure 20: 1961 Aerial showing the high-density development.....	24
Figure 21: 1970 Aerial for the subject site.....	25
Figure 22: 1982 Aerial for the subject site.....	26
Figure 23: Randwick Junction Town Centre Strategy with strategic sites overlayed on heritage and contributory properties.....	64
Figure 24: Key Map for Randwick Junction Town Centre Study Area with sub areas 1 to 13.....	70

1. INTRODUCTION

1.1. Background

City Plan Heritage (CPH) has been engaged by Randwick City Council (RCC) to provide analysis and heritage input for the proposed building envelopes for the Randwick Junction Town Centre, Randwick (study area).

Randwick City Council will be submitting Randwick Junction Town Centre Planning Proposal to the Department of Planning for Gateway Determination. The planning proposal comprise of new building envelopes and controls for Randwick Junction to meet the future need for employment and business generating floor space from the adjoining health and medical precinct. The proposed envelopes have undergone financial feasibility analysis to ensure that future uplift and redevelopment is viable for landowners while respecting the overall heritage character of the town centre.

The key urban design principles that form the basis for the proposed building envelopes include:

- (1) *Preserve and enhance the area's distinctive heritage identity and sense of place;*
- (2) *Focus density and taller buildings in a limited number of major opportunity sites with large floor plates and potential to deliver improved public domain and urban design outcomes;*
- (3) *Maximum street wall height to be three storeys for new buildings in the study area with any fourth storey to be setback by 4m;*
- (4) *New development to incorporate front setbacks along High Street, to create a wide footpath near the Light Rail stop;*
- (5) *Encourage opportunities for through site/midblock pedestrian links, internal courtyards, and new or upgraded public places as part of any comprehensive redevelopment of major opportunity sites;*
- (6) *Encourage active frontages along main streets, continuing down side streets and laneways; and*
- (7) *Seek opportunities to activate and extend retail/commercial activities to laneways.*

This report aims to review the financial feasibility analysis prepared by SGS, including supporting background material; and provide heritage advice in relation to the draft planning controls.

1.2. Context of the Planning Strategy

The following summary provides purpose of the planning strategy and has been extracted from the 'Randwick Junction Town Centre Strategy (August 2020)', prepared by Council Officers.¹

This draft Planning Strategy sets out the vision, strategies and implementation actions to guide the sustainable growth and physical development of the Randwick Junction Town Centre (RJTC) over the next 15 years. It forms the basis for changes to land use and built form controls in the Randwick Local Environmental Plan 2012 (LEP 2012) and the Randwick Development Control Plan 2013 (DCP 2013), together with improvements to the public domain to enhance the liveability, sustainability and economic prosperity of the town centre.

1.3. The Site

Randwick Junction is located in Sydney's Eastern suburbs, 8km from Sydney CBD and 7km from Sydney Airport. Nearby centres include Bondi Junction to the north, and Maroubra Junction to the south. Key

¹ 'Randwick Junction Town Centre Strategy (August 2020)', Randwick City Council, pp. 4.

corridors linking Randwick Junction to the city and other centres include Avoca Street, Anzac Parade and Alison Road. Randwick Junction is an established medium-density residential area within walking distance of the Centre, located in close proximity to the Randwick Health Campus and the University of NSW.

The study area is located within Randwick Junction HCA boundaries with some sites outside the HCA boundary along Arthur Lane, Elizabeth Street and Avoca Street. The study area includes six groups of residential zoned sites identified for inclusion in the Randwick Junction Town Centre. The six groups of properties include²:

- (1) Nos. 119, 121, 123, 125, and 127-129 Alison Road (5 properties currently zoned R3)
- (2) Nos. 1, 3, 5 Botany Street and 1, 3, 5, 7, 9 Silver Street (8 properties in total currently zoned R3)
- (3) Nos 144, 146, 148, 150, 152-154 and 156, 158, 160 and 162 Avoca Street (9 properties in total currently zoned R3)
- (4) Nos 66 and No 68 Arthur Street (2 properties in total currently zoned R3)
- (5) No 56-58, 60 and 62 High Street & No 17 and 19 Clara Street (5 properties in total currently zoned R3)
- (6) Nos 42 and 44 Waratah Avenue, 9 Arthur Lane and 63, 65, 67, 69 Arthur Street (7 Properties in total currently zoned R3)

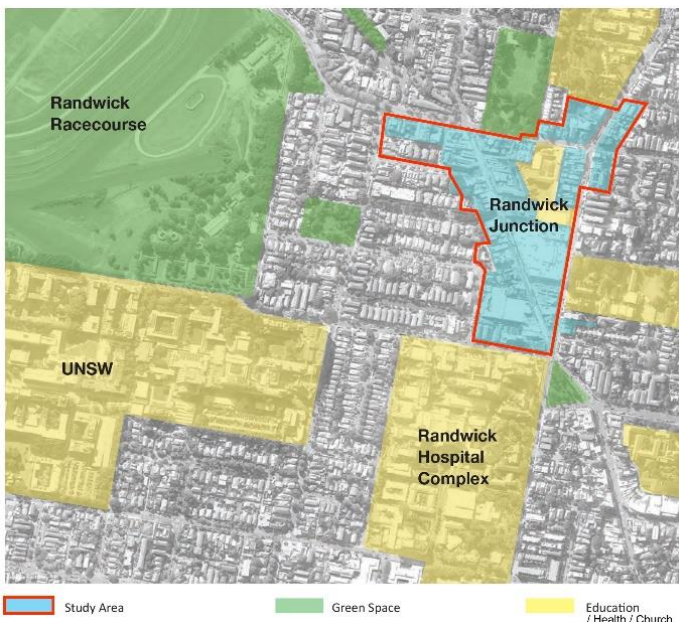


Figure 1: Study Area (indicated in blue) for Randwick Junction Town Centre (Source: Randwick City Council (August 2020), 'Randwick Junction Town Centre Strategy').

1.4. Heritage Listing

The subject area primarily comprises of Randwick Junction Heritage Conservation Area ([C15]) (HCA) listed under Part 2 of Schedule 5 of the Randwick Local Environmental Plan (LEP) 2012. The subject area also includes a number of heritage items of State and local heritage significance listed under Part 1, Schedule 5 of the Randwick (LEP) 2012 and/or State Heritage Register under the NSW *Heritage Act 1977*.

² 'Randwick Junction Town Centre Strategy (August 2020)', Randwick City Council, pp. 11.

NSW Heritage Act, 1977

State Heritage Register

- 'Sandgate', 128 Belmore Road, SHR no. 00067
- 'Randwick Post Office (former) and Jubilee Fountain', 124 Alison Road, SHR no. 01409

Environmental Planning and Assessment Act, 1979

Randwick LEP 2012, Part 1 Heritage items

- 'Federation commercial building', 125–133 Belmore Road, item no. I300
- 'The Star and Garter Inn', 141–143 Belmore Road, item no. I302
- 'Statue of Captain James Cook', 145M Belmore Road, item no. I303*
- 'Late Victorian shop and residence', 194 Avoca Street, item no. I285
- "'Cooks Lodge" 2 storey commercial building', 119-123 Belmore Road, item no. I298
- 'Commercial building', 120-126 Belmore Road, item no. I299
- "'Sandgate'", 128 Belmore Road, item no. I301
- 'Semi-detached pair', 17-19 Clara Street, item no. I316*
- "'Koorowi Flats" 3 storey commercial/residential building', 84-90 Belmore Road, item no. I297
- '3 storey commercial building', 70-82 Belmore Road, item no. I296
- '3 storey Art Deco commercial building', 35-43 Belmore Road, item no. I294
- 'Residential/commercial building', 48-60 Belmore Road, item no. I295
- 'Terraced pair', 152-154 Avoca Street, item no. I281
- 'Terraced pair', 156-158 Avoca Street, item no. I282
- "'Somerset" and "Glastonbury"', 160-162 Avoca Street, item no. I283
- "'Clovelly", "Ilfracombe" and "Torquay", Italianate houses', 146-150 Avoca Street, item no. I279
- 'Coach and Horses Hotel', 147 Avoca Street, item no. I280
- 'Part of Victorian and inter-war building', 139-145 Avoca Street, item no. I278
- 'Victorian and inter-war building (shopfronts with residences above)', 115-137 Avoca Street, item no. I273
- 'Hetta Building, commercial/residential building', 110-116 Avoca Street, item no. I272
- 'Former Randwick Post Office', 124 Avoca Street (206A Alison Road), item no. I274
- 'Jubilee Fountain', 124M Avoca Street, item no. I275*
- 'Commercial building', 126-128 Avoca Street, item no. I276
- 'Commercial building', 130-138 Avoca Street, item no. I277
- 'Stone commercial building', 200 Alison Road, item no. I260
- "'Glanmire", Victorian house', 193 Alison Road, item no. I259*
- "'Seabird", Victorian house', 191 Alison Road, item no. I258*
- "'Rexmere", Victorian terrace', 143 Alison Road, item no. I255
- "'Hillcrest", Victorian terrace', 145-147 Alison Road, item no. I256

NOTE: Items with asterisk () have no changes to their building envelope*

Environmental Planning and Assessment Act, 1979

Randwick LEP 2012, Part 2 Heritage Conservation Areas

- Randwick Junction Heritage Conservation Area ([C15])

Potential Heritage Items proposed for heritage listing as part of the Randwick Council Comprehensive Planning Proposal LEP Review lodged with the Department of Planning in September 2022.

- '1 Belmore Road (Above awning Facade and Awning only), Randwick'
- "'Warrington", 167-171 Alison Road, Randwick'
- "'Montrose", 179-181 Alison Road, Randwick'
- '25 Waratah Avenue, Randwick' (To be include within the curtilage of Heritage Item I295)



Figure 2: Heritage Map for the subject study area (indicated in blue) within its surrounding context. The proposed LEP local heritage listing indicated in green shading (Source: Randwick LEP 2012, Heritage Maps - Sheet HER_002 and Sheet HER_007).

1.5. Methodology

This heritage assessment employs a methodology that is consistent with the guidelines for Heritage Studies prepared by the then NSW Heritage Office and Department of Urban Affairs and Planning. It also uses the system described in the Heritage NSW Manual 'Assessing Heritage Significance' guidelines. The philosophy and process adopted is that guided by *The Burra Charter: The Australia ICOMOS Charter for Places of Cultural Significance, 2013* (Burra Charter).

The first stage of this study was to review the existing studies and the inventory forms for the Randwick Junction HCA and the existing Heritage Items within the area. The review allowed for understanding of the previous rationale in the assessment of the boundaries and the Heritage Items within the area.

Research for this report has adopted a two-stepped approach. Step 1 comprised of desktop research including analysis of the provided documents listed below in *Section 1.6 - Documentation* and the State Heritage Inventory Database for listed items in the study area. Step 2 consisted of a site survey. This document provides the combined findings and recommendations resulting from this approach.

Step 1

Research into the evolution of the planning framework for Randwick Junction Town Centre and its related features was undertaken to gain a better understanding of the place. Further, the relevant statutory instruments, reports pertaining to the brief and the SHR (State Heritage Register) were examined to determine the constraints, opportunities and known heritage values of the Randwick Junction Town Centre Area as well as the heritage items and heritage conservation areas located within and around the study area.

Detailed Character Area Statements, Statements of Significance, Descriptions and Conservation policies for the Randwick Junction Town Centre conservation area and surrounding conservation areas were reviewed as a part of this heritage assessment.

Step 2

A detailed survey of the study area was carried out by Kerime Danis (Director - Heritage) and Asmita Bhasin (Heritage Consultant) on 26 October 2022 with the purpose of photographing and understanding the place. All results are presented in *Section 2.2 - Heritage Items*.

1.6. Documentation

During the preparation of this Heritage Assessment Report, the following documentation has been consulted:

- *Heritage Conservation Area Review Randwick Junction - Belmore Road and Avoca Streets* (September 2015), Randwick, Randwick City Council, Prepared by City Plan Heritage
- *Randwick Junction Town Centre Strategy* (August 2020), Randwick City Council
- *Randwick Junction - Proposed Height of Building Map* (Draft - 31 August 2022), Randwick Junction DCP, Randwick City Council

1.7. Limitations

- A detailed historical and archaeological assessment, including an assessment of Aboriginal cultural heritage values, is not considered necessary at this time and does not form part of the scope of this report. An assessment of the Aboriginal cultural significance of an area can only be made by Aboriginal communities.

- This assessment relates to the resources and documentation described in Section 1.5 - Methodology and Section 1.6 - Documentation. It does not relate to any additional or revised documentation by any party.
- Most recommendations are made on the basis of an external survey. Intrusive methods were not employed.

1.8. Author Identification

The following report has been prepared by Asmita Bhasin, Heritage Consultant (BArch, MURbanism (HeritCons), MICOMOS) and Kerime Danis, Director - Heritage (BArch, MHeritCons (Hons), Associate RAIA, MICOMOS, NMAS), who has also reviewed and endorsed its content.

All current images of the Study Area are taken by City Plan Heritage unless otherwise stated.

2. SITE CONTEXT AND DESCRIPTION

2.1. Site Context

The subject site (Randwick Junction Town Centre) is located in the suburb of Randwick, which is located approximately 6km southeast of the Sydney Central Business District (CBD). It is within a mixed residential and commercial suburb located within the local government area of Randwick City Council.

Randwick Junction Town Centre area is a diverse and vibrant commercial centre characterised by the large concentration of health and medical buildings of the nearby Hospitals precinct and a traditional strip of commercial establishments. The University of NSW and the Randwick Racecourse are located west of the subject site, well served by buses and light rail. Public transport and heavy traffic dominate the streetscapes of the triangular block formed by Belmore Road, Alison Road and Avoca Street. Coogee Beach is also located east of the subject site.

The following images provide an overview of the site's context and current built heritage within the changing nature of the development character and envelopes.



Figure 3: Prince of Wales Hospital complex located opposite subject site along High Street and Avoca Street, view looking southwest.



Figure 4: Randwick Light Rail station along High Street, view looking northeast.



Figure 5: Low-to-medium density development at the corner of Alison Road and Botany Street, view looking southwest.

2.2. Site Description

The subject site comprises of Randwick Junction Town Centre and some additional allotments along Arthur Lane in the west, Alison Road and Elizabeth Street to the northwest, and Avoca Street to the southeast. The subject site includes almost the entire area known as Randwick Junction. Randwick Junction Heritage Conservation Area (HCA) located within the subject site is the only conservation area within Randwick City that is focused solely on a commercial centre. It retains a coherent commercial streetscape character of the nineteenth and twentieth century buildings. The majority of these buildings are located in two distinct groups, the Belmore Road grouping and the “Coach and Horses” grouping (centred on the intersection of Alison Road and Avoca Street).

The majority of the buildings within the commercial strips are two to three storeys Victorian and Federation periods with the Inter-War and Post-War commercial and residential buildings scattered in between. Parapets of these buildings are one of the distinctive characteristics of the area so are the suspended awnings providing a comfortable and protected shopping experience to the locals. Most of the Heritage Items and Contributing Items remain intact at the northern half of the Randwick Junction HCA namely along Alison Road, its intersection with Avoca Street, and Belmore Road. The significant building form and character in the southern half of the Randwick Junction HCA is interrupted by the two large shopping centres creating neutral and detracting elements within the cohesiveness of the traditional commercial strips. Captain Cook Statue at the intersection of Belmore Road and Avoca Street creates a gateway and clue to the historical character and significant building stock of the Randwick Junction HCA.

The development along Belmore Road primarily comprises of double-storey commercial development along the east and double-to-triple storey mixed-use development (commercial and residential apartments) along the west. Most of the development along Belmore Road follow a similar scale and bulk, except for a few contemporary buildings that reflect detracting characteristics. Similarly, along Alison Road, the existing built form reflects both commercial and residential use. Marcellin College is also located on Alison Road, in close proximity to Easts House.

The Randwick Junction HCA has been divided into thirteen (13) blocks to assist in the interpretation of characteristics and ranking of the properties. The information on each block is provided in Section 6.3 of this report.

The following images provide an overview of the site's current physical condition.



Figure 6: Multistorey flat buildings along High Street, opposite Randwick Light Rail Station, view looking northeast.



Figure 7: Wales Medical Centre along High Street, view looking north.



Figure 8: Junction of Avoca Street, High Street and Belmore Road in Randwick, view looking northeast.



Figure 9: Commercial development along Belmore Road, view looking southeast.



Figure 10: Multi-storey early development along Belmore Road, view looking southeast.



Figure 11: Corner of Alison Road and Belmore Road, view looking southeast.



Figure 12: Multi-storey mixed-use development along Belmore Road, view looking southwest.

3. HISTORICAL CONTEXT

3.1. Aboriginal History

Archaeological evidence has estimated that Aboriginal people have survived on this continent for approximately 60,000 years and have lived a nomadic existence, where their spiritual beliefs and practices directly connected to the land. Before the European invasion, the local people moved around the area to hunt and fish. The main component of coastal diet was fish, the men using multi-pronged spears, and the women using hooks and lines. Women also gathered shellfish and plant foods such as fern roots and native figs. However, contemporary Aboriginal society believes that through their dreaming they have always existed on this land and their lifestyle remained unchanged until the arrival of Europeans³.

The Bidjigal and Gadigal people of the Eora Nation are the traditional custodians of the Sydney area. The Bidjigal and Gadigal people were coastal dwelling clans, and they were dependent on the harbour and ocean for providing food. The people of the Eora nation inhabited the current Sydney metropolitan basin. Within this nation there were many different Bands (or clan groups), the Bidjigal and Gadigal people lived in and around the Coogee area⁴. The traditional custodians of the Randwick City area were majority, the Bidjigal people, the Gadigal people and the Muru-Ora-Dial people, a clan which occupied the locality of Maroubra⁵.

It is believed that the inhabitants in Coogee and wider Randwick City area spoke a dialect of the Dharug language which was spoken by half of the 29 Eora Nation clans⁶. The migration in the area dramatically reduced Indigenous population with the introduction of smallpox in the first years of European contact. Additionally, deaths due to diseases, warfare and polluting of local water sources forced locals off their homelands, areas which lead to regrouping of remnant groups⁷. Archaeological and anthropological investigations suggest that the Gadigal people survived⁸.

Remnants of the traditional owners of this area can be found in rock engravings. Rock engravings and paintings were a popular form of artistic expression. Common subjects along the coast were whales, sharks and fish. Early settlers did not record religious beliefs, but lore and religion were of paramount importance in Aboriginal society, expressed through ceremonies, dance, art and song⁹.

Zero Aboriginal sites and places are identified around the subject area (based on the general information generated by the Aboriginal Heritage Information Management System - AHIMS) within a buffer of 50m and 200m. However, three Aboriginal sites were identified within the buffer of 1000m at the southwest of the subject area, around UNSW - Kensington campus, Botany Street and Barker Site.

3.2. Brief History of Randwick

European settlement in the Randwick area did not take off until the nineteenth century as poor soil and swamps made the area an undesirable location.¹⁰ Randwick eventually developed into an English classical

³ Randwick City Council, 'Indigenous history - Aboriginal history of Randwick', accessed [online] randwick.nsw.gov.au/about-us/history/our-story/indigenous-history.

⁴ Randwick City Council (May 2019), 'Coogee and the Bidjigal and Gadigal People', accessed [online] <https://www.randwick.nsw.gov.au/about-us/news/news-items/2019/may/coogee-and-the-bidjigal-and-gadigal-people#:~:text=The%20Bidjigal%20and%20Gadigal%20people%20were%20coastal%20dwelling%20clans%20and,ands%20lines%20to%20catch%20it>.

⁵ Crocker, B. (2019), 'Traditional Custodians of the Land', Firstly, accessed [online] <https://medium.com/firstly/the-mapping-of-the-randwick-city-council-area-specifically-focusing-on-the-local-communities-of-13abfc67b177>.

⁶ Sydney Barani (2013), 'Aboriginal People and Place', accessed [online] <http://www.sydneymarani.com.au/sites/aboriginal-people-and-place/>.

⁷ Crocker, B. (2019), 'Traditional Custodians of the Land', Firstly, accessed [online] <https://medium.com/firstly/the-mapping-of-the-randwick-city-council-area-specifically-focusing-on-the-local-communities-of-13abfc67b177>.

⁸ Ibid.

⁹ Randwick City Council, 'Indigenous history - Aboriginal history of Randwick', accessed [online] randwick.nsw.gov.au/about-us/history/our-story/indigenous-history.

¹⁰ <http://www.randwick.nsw.gov.au/about-council/history/history-overview>

village which attracted Sydney's social elite during the mid-nineteenth century. The attractiveness of Randwick during these early formative years was a result of two important factors. The first being the decision by Bishop Barker of Sydney to locate his new premises "Bishopscourt" at Randwick rather than Newtown, which was his original intention. The Bishop's decision developed as a result of a visit he took to Randwick in 1855 in order to visit the Commissioner of Lands for NSW, Simeon Henry Pearce and his wife Alice. The prosperity of the Pearce family was clearly visible to the Bishop along with the desirability of the land which had both ocean and city views.¹¹

Pearce was a driving force in the area, and it was his mission to attract such members of society to the area, his motivation being to change Randwick from a population consisting of mainly modest market gardeners.¹² Pearce, who immigrated to Australia from England, is renowned for naming the suburb of "Randwick" which is derived from his hometown of Randwick in Gloucestershire, England.¹³

The second factor in the development of Randwick in the late nineteenth century was the establishment of the Randwick Destitute Children's Asylum in 1858 (now part of the Prince of Wales Hospital). Built by colonial architect Sir Edmund Blackett, the grand sandstone buildings of the Asylum became a landmark for Randwick and are still considered as such today. This building, along with an ideal topography, set the scene for the grand houses Pearce envisaged for the area.

Eventually, the grand and ornate buildings Pearce so desperately wanted built were constructed and the area developed into a beautiful collection of ornate Victorian homes and boulevards. Randwick is still considered a highly desirable suburb to this day.¹⁴ Industry also developed in the area with the most significant site being the Tramway Workshops.¹⁵

In 1859 the municipality of Randwick was declared (the first municipality in NSW¹⁶) and in 1865 St Jude's Anglican Church was built on Avoca Street.¹⁷ The construction of the church was imperative to the establishment of Randwick village and became the centre of village life.¹⁸ Eventually the population of Randwick steadily increased and by 1911 the population numbered over 18,000.¹⁹ Randwick Racecourse, one of the most iconic landmarks in the area, opened in 1833, surprisingly early in the colony's history.²⁰

3.3. Development of Randwick Junction Heritage Conservation Area

The Randwick Junction HCA is associated with the some of the earliest settlement in Randwick. Much of the area was crown land before it was granted to early landowners J. Pearce, Simeon Pearce, Alexander Macarthur and Samuel Hebblewhite.

The earliest commercial development within the Randwick Junction HCA occurred at the corner of Avoca Street and Alison Road where the Coach and Horses Hotel (originally operated as the Vauxhall Gardens for a few years before changing its name) was established in 1856.²¹ This hotel acted as a temporary post office and also offered a horse drawn carriage service to Sydney.²²

With the introduction of the tram way in the 1880s including a tram workshop brought large numbers of settlers to the area leading to the original land grant owners to subdivide and sell their land to property

¹¹ <http://www.randwick.nsw.gov.au/about-council/history/historic-places/historic-suburbs>

¹² Ibid

¹³ Ibid

¹⁴ Ibid

¹⁵ Ibid

¹⁶ <http://www.randwick.nsw.gov.au/about-council/history/our-story/timeline>

¹⁷ <http://www.randwick.nsw.gov.au/about-council/history/history-overview>

¹⁸ <http://www.randwick.nsw.gov.au/about-council/history/historic-places/historic-suburbs>

¹⁹ <http://www.randwick.nsw.gov.au/about-council/history/history-overview>

²⁰ <http://www.randwick.nsw.gov.au/about-council/history/historic-places/historic-suburbs>

²¹ P. Spearrit, Sydney's Century, p.68.

²² F. Pollon, Sydney Suburbs, p.21.

developers. It is in this period when the Randwick Junction HCA experience its greatest period of development establishing the property layout seen today.

The inter war and post war years were also a period of rapid development for Randwick. The 1930 aerial photo indicates much of the eastern side of Belmore Road is developed for residential land use rather than commercial land use. The 1943 aerial photo shows greater commercial density on the western side of Belmore Road and southern section enclosed by High and Clara Streets. From 1943 to 1961 there does not appear an increase in density within the Randwick Junction HCA. The 1982 aerial photo show the construction of a large supermarket along with office blocks in the south of the Randwick Junction HCA. The Marcellin College (a catholic Secondary School in the north of the town centre) oval was built over in this period.

The present day aerial shows the construction of a large mall complex between the southern end of Belmore Road and Avoca Street. Furthermore, the development of Marcellin College continued in the 1980s.

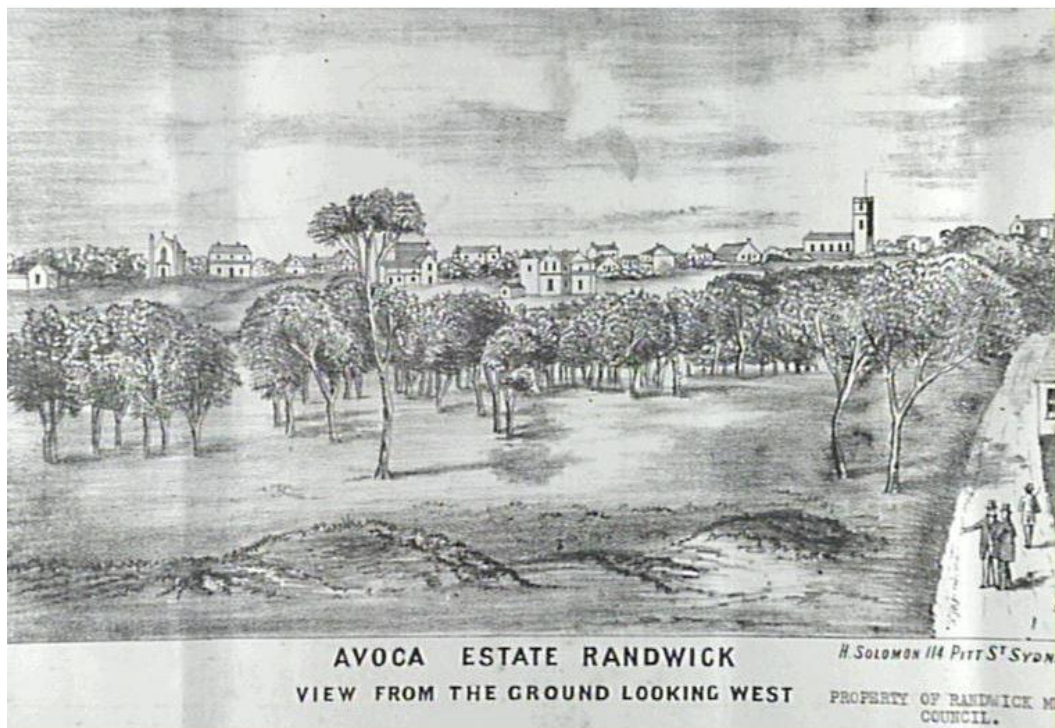


Figure 13: The above c.1880's drawing by H. Solomon shows the Avoca Estate, Randwick looking west towards the township of Randwick and current conservation area. Avoca Street where part of the current HCA lies is named after this estate. (Source: Randwick Photo Gallery <http://www.photosau.com.au/Randwick/scripts/Navigate.asp?mode=full&start=7&pDocs=21&pSrch=1> Retrieved 01/06/15)



Figure 14: The above watercolour by A. Fenton Spencers depicts the corner of Alison Road and Avoca Street c1895. The corner where J.T. Hunter Grocer is depicted is no longer present. The site is now occupied by a triple storey Victorian Building. (Source: National Library of Australia Ref: ON 4 Box 56 No 248).



Figure 15: Captain Cook Statue at corner of Avoca Street and Belmore Road in 1883
(Source: <http://trove.nla.gov.au/version/206437955>)

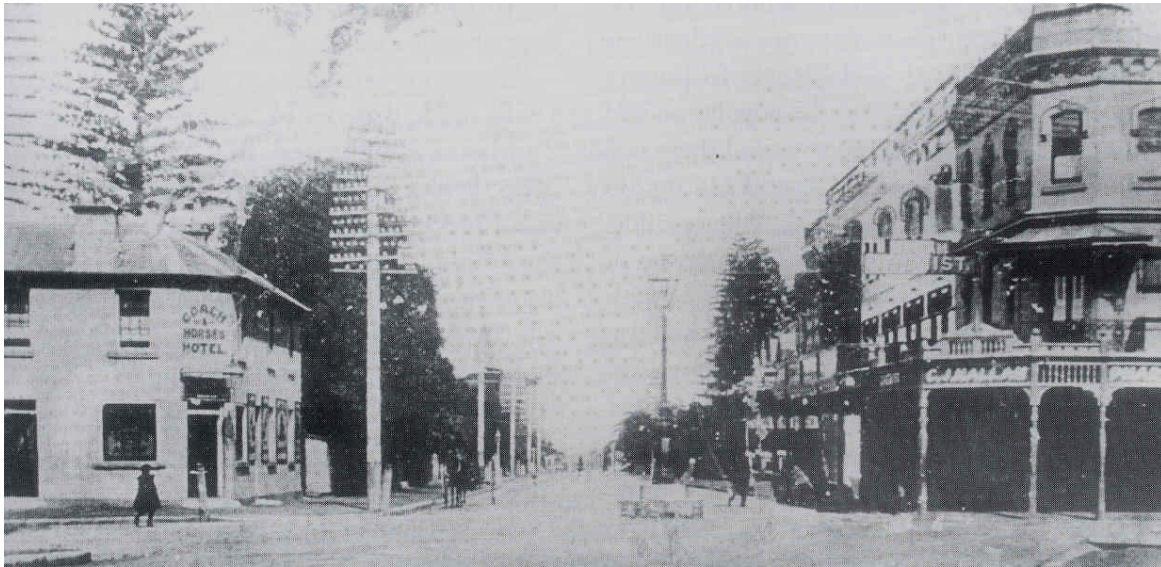


Figure 16: Avoca Street looking South c.1900. The Randwick Junction HCA boundary is along the right side of Avoca Street. (Source: National Library of Australia Ref: ON 4 Box 56 No 248).



Figure 17: The eastern side of Belmore Road, Randwick c.1925 between Short and Avoca Streets. (Randwick Photo Gallery)



Figure 18: 1930 Aerial of Randwick Junction. Note the underdeveloped eastern side of Belmore Road. (Source: Land & Property Information)



Figure 19: 1943 Aerial of the subject site showing an increase in development along Belmore Road. (Source: Land & Property Information, SIX Maps)



Figure 20: 1961 Aerial showing the high-density development now occurring along Belmore Road and surrounding area. (Source: Land & Property Information)



Figure 21: 1970 Aerial for the subject site showing the increased commercial and residential development especially on the periphery of the HCA. (Source: Land & Property Information)



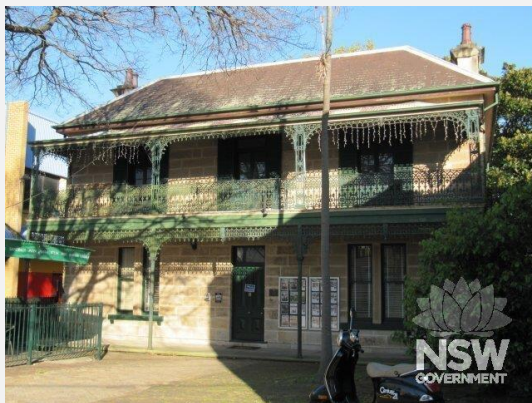
Figure 22: 1982 Aerial for the subject site showing the increased commercial and residential development especially on the periphery of the HCA. (Source: NSW Historical imagery, accessed March 2023).

3.4. Heritage Items

The following section provides a breakdown of the heritage items present within the subject site, identifying their significance, development potential and high-level site specific policies which should guide any future development related to these items. The specific policies do not consider the specifics of the current Randwick Junction Town Centre Strategy (RJTCs) for an item. The proposed strategy is discussed in Section 5. The statements of significance and Specific Policies for these items have been extracted from the individual State Heritage Inventory (SHI) form for each heritage item²³.

Sandgate (SHR no. 00067)

128 Belmore Road



Source: State Heritage Inventory (SHI) Form

Statement of Significance

Sandgate constructed circa 1879 is a two storey sandstone residence. It is associated with Simeon Pearce and the Pearce family who were prominent in the early society and politics of Randwick Municipality. It was designed as a 'Gentleman's residence' and the house retains its original character. (Heritage Branch files)²⁴

Specific Policies

- Recommended Management - Carry out interpretation, promotion and/or education
- Recommended Management - Prepare a maintenance schedule or guidelines
- Recommended Management - Produce a Conservation Management Plan (CMP)

Sandgate (Item no. I301)

128 Belmore Road

Statement of Significance

Sandgate was built circa 1870 as his family residence by prominent early citizen Simeon Pearce who was the first Mayor of Randwick. The house is an excellent example of mid-Victorian 'Gentleman's residences' and is of particular interest in design for its combination of a Georgian building form with Victorian Filigree embellishments. The building is an important contributor to the streetscape of Belmore Road. 'The Proud of our Elders' mural also contributes

²³ Heritage NSW, State Heritage Inventory database, accessed [online]

https://www.hms.heritage.nsw.gov.au/App/Item/SearchHeritageItems?_ga

²⁴ 'Sandgate', State Heritage Inventory (SHI) Form, Heritage Item ID 5045450, accessed [online]

<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=5045450>

Sandgate (Item no. I301)

128 Belmore Road



Source: State Heritage Inventory (SHI) Form

to the significance of Sandgate through its high social and aesthetic value. Sandgate is also representative of Randwick City Council's effort for the preservation of a heritage asset as part of development approval process²⁵

Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes.

Randwick Post Office (former) and Jubilee Fountain (SHR no. 01409)

124 Alison Road

Statement of Significance

Randwick Post Office is significant at a State level for its historical associations, strong aesthetic qualities and social value.

Randwick Post Office is historically significant because it is associated with the NSW Government Architect Walter Liberty Vernon, and is part of an important group of works by Vernon, a key practitioner of the Federation Free style. Randwick Post Office is also associated with the pattern of subdivision and the development of communications services in the Randwick area.

²⁵ 'Sandgate', State Heritage Inventory (SHI) Form, Heritage Item ID 5045450, accessed [online]
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=5045450>

Randwick Post Office (former) and Jubilee Fountain (SHR no. 01409)

124 Alison Road



Source: State Heritage Inventory (SHI) Form

Randwick Post Office is aesthetically significant because it is a fine example of the Federation Free style, with strong visual appeal. It is located on a prominent corner site and makes a significant contribution to the streetscape of the Randwick civic precinct.

Randwick Post Office is also considered to be significant to the community of Randwick's sense of place.

(Andrew Ward & Associates and Clive Lucas Stapleton and Associates, 2000)²⁶

Specific Policies

Preparation of a Conservation Management Plan which addresses the curtilage issues of the site and includes the outbuildings.

Former Randwick Post Office (Item no. I274)

124 Avoca Street

Statement of Significance

Randwick Post Office is of State significance for its historical associations, strong aesthetic qualities and social value.

Randwick Post Office is historically significant because it is associated with the first NSW Government Architect Walter Liberty Vernon, and is part of an important group of works by Vernon, a key practitioner of the Federation Free style. Randwick Post Office is also associated with the pattern of subdivision and the development of communications services in the Randwick area.

²⁶ 'Randwick Post Office (former) and Jubilee Fountain', State Heritage Inventory (SHI) Form, Heritage Item ID 5051250, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=5051250>

Former Randwick Post Office (Item no. I274)

124 Avoca Street



Source: Google Street View (November 2022).

Randwick Post Office is aesthetically significant as a fine example of the Federation Free Classical style, with strong visual appeal. It is located on a prominent corner site and makes a significant contribution to the streetscape of the Randwick civic precinct.

Randwick Post Office is also considered to be significant to the Randwick community's sense of place due to its communications services and use by the Ted Noff's Foundation. (Andrew Ward & Associates and Clive Lucas Stapleton and Associates, 2000)²⁷.

Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes.

Jubilee Fountain (Item no. I275)

124M Avoca Street

Statement of Significance

Part of Randwick Post Office. Refer to 2310067.²⁸

²⁷ 'Former Randwick Post Office', State Heritage Inventory (SHI) Form, Heritage Item ID 2310067, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310067>

²⁸ 'Jubilee Fountain', State Heritage Inventory (SHI) Form, Heritage Item ID 2310055, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310055>

Jubilee Fountain (Item no. I275)

124M Avoca Street



Source: State Heritage Inventory Form

Specific Policies -
Not Available on the item's SHI form

Golding House (Item no. I286)

203-209 Avoca Street



Source: State Heritage Inventory (SHI) Form.

Statement of Significance

*Building with the bulk of its original façade intact.
Situated at a key corner location.²⁹*

Specific Policies -
Not Available on the item's SHI form

²⁹ 'Golding House', State Heritage Inventory (SHI) Form, Heritage Item ID 2310076, accessed [online]
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310076>

Late Victorian Shop and Residence (Item no. I285)

194 Avoca Street



Source: State Heritage Inventory (SHI) Form.

Statement of Significance

Constructed in 1894 by Edward Hearne, the building at 194 Avoca Street, Randwick is evidence of commercial and residential development in Randwick in the late Victorian era when the highly elaborate terraces and commercial buildings were in demand. This two storey building is one of a few still surviving late Victorian shop and residence developments in the Randwick Junction shopping area. It is an excellent example of its type featuring architectural elements of the classical order including stuccoed masonry construction with imitation ashlar. The building features a segmental arched door opening with transom on Avoca Street frontage with original shop windows, segmental arched windows and a door. A series of round arched window openings and a balustrade balcony on first floor, moulded string courses, moulded architraves/hoods with keystone to balcony arch, and a high parapet with two pediments with floral decoration and a corbelled chimney acting as a skyline element with masonry balustrades between the pediments and the chimney. The entablature features moulded sunken and embossed plates to Avoca Street frontage and the splayed corner then steps down along Short Street elevation. The building has a prominent position and strong visual presence at the intersection of Avoca and Short Streets.³⁰

Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes.

³⁰ 'Late Victorian Shop and Residence', State Heritage Inventory (SHI) Form, Heritage Item ID 2310075, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310075>

Terraced Pair (Item no. I281)

152-154 Avoca Street



Source: State Heritage Inventory (SHI) Form.

Statement of Significance -

Not Available on the item's SHI form

Specific Policies -

Not Available on the item's SHI form

Terraced Pair (Item no. I282)

156-158 Avoca Street



Source: Google Street View (November 2022).

Statement of Significance

Special interest for its unusual Gothic and Italianate influences. Complex roofing and veranda/balcony form of special note. Part of important streetscape group.

Constructed c.1890 this pair of terraces are of local significance as evidence of the residential development pattern and characteristics in Randwick in the transition period from Victorian to Federation period. The two terraces are of outstanding and rare examples of their type with particular interest due to their unusual Gothic and Italianate influences presented in the elaborate detailing, embellishment and form of the terraces. Key elements of this two storey stuccoed pair are large faceted bay windows at each level with decorative half-timbered flying gables above; verandas and balconies following the line of the bay windows, creating an interesting shape and complex corrugated iron roofs over the faceted bays and balconies; cast iron filigree/lacework, posts, friezes, fringes and brackets; and palisade

Terraced Pair (Item no. I282)

156-158 Avoca Street

fencing with tapered pillars. They contribute significantly to the streetscape of Avoca Street and the built heritage fabric of Randwick³¹.

Specific Policies -

Not Available on the item's SHI form

Somerset and Glastonbury (Item no. I283)

160-162 Avoca Street



Source: Google Street View (November 2022).

Statement of Significance

Constructed in the Federation Filigree architectural style, these two terraces are important contributors to the streetscape of Avoca Street representing the Federation era residential development pattern in Randwick. The terraces maintain their overall architectural integrity and together with their neighbouring terraces at 156-158 Avoca Street they significantly add to the historical urban fabric of Randwick. Common architectural features of note include the Dutch gabled parapet with Roman skyline ornament (number 162 missing top spike of the ornament); cast iron filigree balustrade, fringe, brackets and posts; asymmetrical façade arrangement with triple Gothic style arched window with Corinthian style masonry mullions, and arched door to ground floor and two French doors with transoms to upper floor³².

Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes

³¹ 'Terraced Pair', State Heritage Inventory (SHI) Form, Heritage Item ID 2310072, accessed [online]
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310072>

³² 'Somerset and Glastonbury', State Heritage Inventory (SHI) Form, Heritage Item ID 2310073, accessed [online]
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310073>

Clovelly, Ilfracombe, Torquay, Italianate Houses (Item no. I279)

146-150 Avoca Street



Source: Google Street View (November 2022).

Statement of Significance

This group of houses at 146-150 Avoca Street, Randwick are of local heritage significance for their historical, aesthetic and representative values. They are outstanding group of three grand scale terrace houses demonstrating the Federation Boom Style quality development that occurred at the transition period from Victorian to Federation period in Randwick. The terrace houses are in an excellent state of preservation and feature Italianate architectural elements including elaborate moulding throughout. Of particular note are the asymmetrical facades created by the large elliptical arches on one side and the smaller round arches on the other, repeated on each level of each building. The Dutch gabled pediments depict the name of each building and feature intricate moulding detail in the Corinthian order. The group has an excellent contribution to the streetscape of Avoca Street.³³

Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes

³³ 'Clovelly, Ilfracombe, Torquay, Italianate Houses', State Heritage Inventory (SHI) Form, Heritage Item ID 2310069, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310069>

Coach and Horses Hotel (Item no. I280)

147 Avoca Street



Source: Google Street View (November 2022).

Statement of Significance

The Coach and Horses Hotel is of historic significance to the local area as one of the first two hotels to be built at Randwick, both in the 1850s at which time the settlement at Randwick was beginning to develop. The Hotel was also the location of the first Randwick to Sydney horse drawn omnibus in the late 1850s. The Hotel is a good representative example a late Victorian Regency style hotel building. It makes an important contribution to the character of the Randwick Junction Conservation Area, occupying an important corner site and complementary in scale and character with the surrounding development of mostly late Victorian period. The site is also likely to have archaeological potential as it encompasses the original two storey sandstone hotel building³⁴.

Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes

³⁴ 'Coach and Horses Hotel', State Heritage Inventory (SHI) Form, Heritage Item ID 2310070, accessed [online]
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310070>

Part of Victorian and inter-war building (Item no. I278)

139-145 Avoca Street



Source: Google Street View (November 2022).

Statement of Significance

Constructed c.1925 the Cassel-Flats building is of historical and aesthetic significance as part of the Inter-War period flat development in Randwick responding to the population growth and the demand for increased residential accommodation at the time. The building is a very good and a prominent example of an Inter-War style flat building with a well-articulated façade and intact joinery detailing. Many original internal features, including apartment configuration, appear to remain intact. Contrasting dark and polychromatic brick and the white painted façade fenestration, the two enclosed faceted balconies with masonry stuccoed decorative solid balustrade, continuous deep window hood supported on decorative brackets, and centrally located arch on the simply articulated parapet with brown chevron detailing are key characteristics of the Cassel-Flats building. These aspects all contribute to the significance of the building. The streetscape setting further contributes to the significance of the building as one of the group of items that provide a characteristic gateway to Avoca Street and Alison Road.³⁵

Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes

³⁵ Part of Victorian and inter-war building', State Heritage Inventory (SHI) Form, Heritage Item ID 2310508, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310508>

Victorian and inter-war building (shopfronts with residences above) (Item no. I273)

115-137 Avoca Street



Source: Google Street View (November 2022).

Statement of Significance

This group of eleven commercial and residential buildings at 115-137 Avoca Street, Randwick are of local significance encapsulating the Victorian and Inter-War periods of commercial development growth in the Randwick municipality. Although they have varying styles and period of construction they represent an unbroken group of buildings that together demonstrate a traditional shopping strip with a strong streetscape contribution. Each building demonstrates key architectural characteristics of their style and are considered to have local aesthetic significance in this regard. Common architectural elements include moulded string courses and cornices, stucco finish, arched windows some with decorative moulded hoods, and sills supported on dentilated brackets. Numbers 117-121 and 137 have a more elaborated parapets while the others have simpler parapet detailing³⁶.

Part commercial uses at ground floor level fronting Avoca Street (hence awnings, shop windows, etc. have been removed). This building appears to have been commercial on the first floor level, too.

Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes. The signage on 115 is considered intrusive and consideration should be given to more sympathetic colour schemes and form of signage for advertising without obscuring the above awning facades. The air conditioning units at 117-135 also detract from the architectural features of the buildings. Consideration should be given to placing these at the rear elevation (Lingard Street).

³⁶ 'Victorian and inter-war building (shopfronts with residences above)', State Heritage Inventory (SHI) Form, Heritage Item ID 2310066, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310066>

Hetta Building, commercial/residential building (Item no. I272)

110-116 Avoca Street



Source: Google Street View (November 2022).

Statement of Significance

Hetta Buildings at 110-116 Avoca Street is of local significance for its ability in illustrating Edwardian residential and commercial development history of Randwick in the mid to late Federation period. This residential and commercial terrace style building is a very good example of creative architecture and façade composition using various decorative elements of the classical order with Edwardian embellishment in a harmonious manner. The façade combines face brick, roughcast render, polychromatic green glazed tiles, egg-shaped ovolo moulded plaster, series of three-row arches with moulded keystones that extend up to the parapet, moulded string course and cornice, Italianate balusters at the top of the parapet, brick pilasters dividing the façade into five bays, and a roughcast pediment with embossed lettering, dentilated moulding and scroll brackets. Hetta Buildings is one of few surviving intact residential and commercial Edwardian terraces in the municipality³⁷.

Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes.

³⁷ 'Hetta Building, commercial/residential building', State Heritage Inventory (SHI) Form, Heritage Item ID 2310065, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310065>

Commercial building (Item no. I276)

126-128 Avoca Street



Source: Google Street View (November 2022).

Statement of Significance

The building at 126-128 Avoca Street has historical significance as one of the key items dating from the Victorian mixed commercial and residential development of the late 1880s in Randwick. It represents some of the earliest residential accommodation in the area. It is of aesthetic significance as an excellent intact example of Victorian Italianate architectural style above awning level with a landmark value as one of four gateway items (including the Coach and Horses Hotel, Randwick Post Office and the Cassel Flats) defining the intersection of Avoca Street and Alison Road. Key architectural characteristics of the building include vertically proportioned a series of double-hung rectangular windows with moulded hoods to first floor level, segmental arched also double-hung windows with moulded hoods to second floor level; a faceted balcony with cast iron posts, lacework balustrade and concave corrugated iron awning to the first floor splayed corner; and a balustraded parapet with piers concealing the roof and ornately moulded cornice with dentils crowning this architecturally appealing corner building.³⁸

Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes.

³⁸ 'Commercial building', State Heritage Inventory (SHI) Form, Heritage Item ID 2310068, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310068>

Commercial building (Item no. I277)

130-138 Avoca Street



Source: Google Street View (November 2022).

Statement of Significance

130-138 Avoca Street, Randwick is of local historic and aesthetic significance for its demonstration of one of the earliest apartment accommodation in the area and for its Victorian Free Classical architectural style with strong visual appeal. This three storey building is an excellent example of the Victorian Free Classical style with skilfully executed decorative architectural elements giving the building its impressive and repetitive façade fenestration. The building together with its adjoining buildings has a landmark value as part of the gateway items (including adjoining 126-128 Avoca Street, the Coach and Horses Hotel, Randwick Post Office and the Cassel – Flats) defining the intersection of Avoca Street and Alison Road³⁹.

Specific Policies -

Not Available on the item's SHI form

³⁹ 'Commercial building', State Heritage Inventory (SHI) Form, Heritage Item ID 2310459, accessed [online]
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310459>

Stone Commercial Building (Item no. I260)

200 Alison Road



Source: Google Street View (November 2022).

Statement of Significance

The building at 200 Alison Road, Randwick is of local heritage significance as part of Randwick's commercial history, operating as the Bank of Commerce from 1915 and the Bank of NSW from 1931. Designed by renowned architectural firm Robertson and Marks, the erection of this building compliments the growth that occurred in the area during the early 20th century. The design of the building is incredibly individual and appears to not proscribe to any particular period fashion. It is therefore of special architectural interest. As such, the façade is a unique design which boast a symmetrical façade contributing to the streetscape of Alison Road and making it to stand out between its neighbouring properties. The building is also unique in employing a stone façade to an essentially face brick two storey building with an attic. Its façade composition is well executed in a simple but elegant way⁴⁰.

Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes.

⁴⁰ 'Stone Commercial building', State Heritage Inventory (SHI) Form, Heritage Item ID 2310016, accessed [online]
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310016>

"Glanmire", Victorian House (Item no. I259)

193 Alison Road



Source: Google Street View (November 2022).

Statement of Significance

Glanmire at 193 Alison Road, Randwick is one of a pair of outstanding late Victorian houses which are the oldest and best survivors of the former Brisbane Estate illustrating the Boom Style architecture in Randwick. The facade of the terrace is hardly altered and substantially reinforces the nineteenth century townscape of the immediate environments of Alison Park, and the Avoca Street/Alison Road intersection. Glanmire has extensive decorative timberwork particularly on gable and around doors. The cast iron posts, lacework balustrade, fringe and brackets, and palisade fence are all original. The terrace significantly contributes to the aesthetic quality of the Alison Street streetscape. The terrace has been part of Marcellin College, one of Randwick's major twentieth century institutions, since 1967 adding to its social significance.⁴¹

Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes.

⁴¹ 'Glanmire, Victorian House', State Heritage Inventory (SHI) Form, Heritage Item ID 2310015, accessed [online]
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310015>

"Seabird", Victorian House (Item no. I258)

191 Alison Road



Source: Google Street View (November 2022).

Statement of Significance

Seabird at 191 Alison Road, Randwick is one of a pair of outstanding late Victorian houses which are the oldest and best survivors of the former Brisbane Estate illustrating the Boom Style architecture in Randwick. The facade of the terrace hardly altered and substantially reinforce the nineteenth century townscape of the immediate environments of Alison Park, and the Avoca Street/Alison Road intersection. Seabird has extensive decorative timberwork particularly on gables and around doors. The cast iron posts, lacework balustrade, fringe and brackets, and palisade fence are all original. The terrace significantly contributes to the aesthetic quality of the Alison Street streetscape. The terrace has been part of Marcellin College, one of Randwick's major twentieth century institutions, since 1967⁴².

Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes.

⁴² 'Seabird, Victorian House', State Heritage Inventory (SHI) Form, Heritage Item ID 2310014, accessed [online]
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310014>

"Rexmere", Victorian terrace (Item no. I255)

143 Alison Road



Source: State Heritage Inventory (SHI) Form.

Statement of Significance

Rexmere is amongst the oldest surviving buildings in Alison Road and contributes to an understanding of the development of the Randwick Junction. It was constructed in a similar style and at approximately the same time as the Grand Avonmore Terraces in The Avenue, Randwick which are listed on the register of the National Estate. The building has aesthetic significance and compliments the surrounding heritage items within the streetscape, for example, the Victorian Italianate houses No.s 126, 128 and 130 Alison Road, Randwick diagonally opposite the Labour Club. This terrace is representative of the grand buildings of the speculative boom in Randwick Junction around the turn of the Century. The building is a fine example of its type and is becoming increasingly rare within the City of Randwick and the Eastern Suburbs in general.⁴³.

Specific Policies -

Not Available on the item's SHI form

"Hillcrest", Victorian terrace (Item no. I256)

145-147 Alison Road

Statement of Significance

The building has aesthetic significance and compliments the surrounding heritage items within the streetscape, for example, the Victorian Italianate houses No.s 126, 128 and 130 Alison Road, Randwick diagonally opposite the Labour Club. This terrace is representative of the grand buildings of the speculative boom in Randwick Junction around the turn of the Century. The building is a fine example of its type and is becoming increasingly rare within the City of Randwick.⁴⁴.

⁴³ 'Rexmere, Victorian Terrace', State Heritage Inventory (SHI) Form, Heritage Item ID 2310431, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310431>

⁴⁴ 'Hillcrest, Victorian Terrace', State Heritage Inventory (SHI) Form, Heritage Item ID 2310432, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310432>

"Hillcrest", Victorian terrace (Item no. I256)
145-147 Alison Road



Source: State Heritage Inventory (SHI) Form.

Specific Policies -

Not Available on the item's SHI form

Federation Commercial Building (Item no. I300)
125-133 Belmore Road



Source: Google Street View (November 2022).

Statement of Significance

Constructed c.1897 this group of five two-storey terraces at 125-133 Belmore Road are of historical significance as a series of Federation commercial/residential buildings at the southern end of Randwick Junction, demonstrating the development type and pattern at beginning of the Federation period. Although they have been modified the above awning facades of the terraces feature classically detailed architectural elements including moulded segmental arched balconies, rusticated piers with corbel tops and ornate belt course. They make significant contribution to the streetscape of Belmore Road providing evidence of the changing nature of development and a move towards terrace development⁴⁵.

These heritage items have sustained considerable dilapidation at the rear of their respective property facing Avoca Street, which have reduced their level of integrity and significance.

Specific Policies

⁴⁵ 'Federation Commercial Building', State Heritage Inventory (SHI) Form, Heritage Item ID 2310101, accessed [online]
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310101>

Federation Commercial Building (Item no. I300)

125-133 Belmore Road

This group of five terraces should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the respective terrace (or the group) prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes. Unsympathetic cluttering should be removed when opportunity arises. A consistent and sympathetic colour scheme should be established for the group to maintain integrity and continuity.

The Star and Garter Inn (Item no. I302)

141-143 Belmore Road



Statement of Significance

One of Randwick's earliest buildings. Considerable historic interest as former school, inn, shop and residence. Considerable streetscape interest for its tower, a focal point in an historic townscape precinct.

Constructed in 1859, the "Star and Garter Inn" is of local significance as one of Randwick's earliest buildings. It has historic significance as former school, inn, shop and residence. The building also has aesthetic significance for its streetscape quality as an early and rare Georgian style stone building with Victorian façade added on corner frontage. Its stone tower with castellated top is a focal point in an historic townscape precinct. Other notable features include tooled finish stone walls with ashlar laid in horizontal band defining the separation between the ground and first floor levels, stone quoins, and vertically proportioned metal framed multi-pane double-hung windows with smooth finished stone sills.⁴⁶

Specific Policies -

Not Available on the item's SHI form

⁴⁶ 'The Star and Garter Inn', State Heritage Inventory (SHI) Form, Heritage Item ID 2310078, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310078>

Statue of Captain James Cook (Item no. I303)

145M Belmore Road



Statement of Significance

Unveiled in 1874, the statue of Captain James Cook at the corner of Belmore Road and Avoca Street is considered to be the first statue of Cook to be erected in Australia. It has historical and associational significance as it was carved by Walter McGill - a sculptor and monumental mason initially worked in Port Fairy, Victoria, from the mid-1850s until the early 1860s, then lived at Maitland, NSW, and was working in Sydney by 1864. After moving to Sydney, McGill carved many stone works in Randwick including the St. Jude's Fountain and the statue of Captain Cook as well as other works throughout Sydney's notable buildings and parks. The statue is considered to have social significance as an iconic historical monumental feature in Randwick. The statue also has aesthetic significance, providing an iconic gateway entrance to the intersection of Avoca Street and Belmore Road⁴⁷.

Specific Policies

The statue should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the statue prior to any major works being undertaken. Archival photographic

recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes.

⁴⁷ 'Statue of Captain James Cook', State Heritage Inventory (SHI) Form, Heritage Item ID 2310094, accessed [online]
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310094>

"Cooks Lodge" two-storey commercial building (Item no. I298)

119-123 Belmore Road



Source: Google Street View (November 2022).

Statement of Significance

The buildings at 119-123 Belmore Road, Randwick have historical and aesthetic significance representing a continuing process of suburbanisation within the Randwick municipality demonstrating the commercial activities in the late Victorian period along southern end of the road. Number 119 is a rare example of Victorian development in the area due to its coat of arms detailing on the parapet which reflects the building's historical use as a Post Office between 1878 and 1895. The buildings are considered to be of aesthetic significance commanding their corner location with their related architectural detailing⁴⁸.

Specific Policies

The buildings should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the buildings prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes.

Commercial Building (Item no. I299)

120-126 Belmore Road



Source: Google Street View (November 2022).

Statement of Significance

The group of four two storey commercial buildings at 120-126 Belmore Road, Randwick is of local significance as and evidence of Inter-War traditional shopping strip with identical above awning facades. Designed in the Art Nouveau style, the group is an important aesthetic contributor to the historical commercial strip streetscape of Belmore Road. The intact Inter-War Art Nouveau architectural style elements of the facades include curved parapets, pilasters with corbelled tops, and ribbon windows with

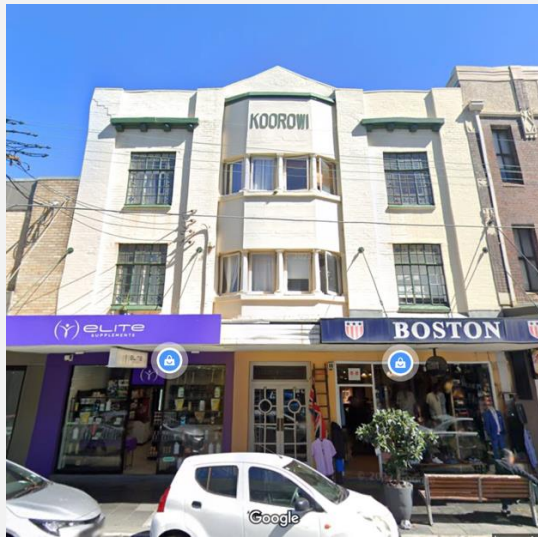
⁴⁸ 'Cooks Lodge, two-storey commercial building', State Heritage Inventory (SHI) Form, Heritage Item ID 2310099, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310099>

Commercial Building (Item no. I299) 120-126 Belmore Road	
	<p><i>hoods supported on corbelled brackets and block modillions. Of particular note is shop 126 which includes some original Art Nouveau leadlight glazing. The external wall of number 126 features the mural 'Proud of our Elders', which is of high social significance and enhances the overall value of this commercial building group⁴⁹.</i></p>
	<p>Specific Policies</p> <p><i>This group of four buildings should be retained and conserved. The shopfront of number 126 should be preserved as an intact example of Art Nouveau style in particular the leadlight glazing. A Heritage Assessment or Heritage Impact Statement should be prepared for the buildings prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes.</i></p>

⁴⁹ 'Commercial building', State Heritage Inventory (SHI) Form, Heritage Item ID 2310100, accessed [online]
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310100>

"Koorowi Flats" 3 storey commercial/residential building (Item no. I297)

84-90 Belmore Road



Source: Google Street View (November 2022).

Statement of Significance

Koorowi Flats building is of local significance as an Inter-War flat building designed in the Art Deco architectural style demonstrating the development pattern and demand for apartment buildings in response to population growth at the time. The building together with adjoining the building makes significant contribution to the streetscape of Belmore Road. The Art Deco style decorative façade fenestration features faceted central balcony bay with Doric balusters, embossed Koorowi lettering, stucco moulded hoods with dentil supports, and multi-paned windows with decorative leadlight glazing. The main entrance door to Belmore Road is also of aesthetic significance. While the rear façade maintains its original decorative face brickwork the front façade has been painted. Paintwork, does not however reduce its significance and contribution to the Inter-War building stock of Randwick.⁵⁰

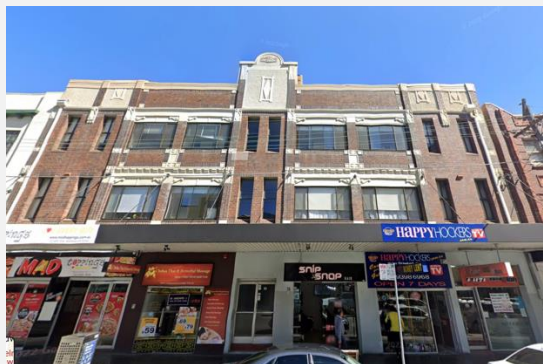
Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes. If opportunity arises the paint over the façade should be removed and the tuck-pointed face brick finish should be reinstated.

⁵⁰ "Koorowi Flats' 3 storey commercial/residential building', State Heritage Inventory (SHI) Form, Heritage Item ID 2310098, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310098>

Three-storey Commercial Building (Item no. I296)

70-82 Belmore Road



Source: Google Street View (November 2022).

Statement of Significance

70-82 Belmore Road is of local significance as an excellent example of Inter-War Art Deco architecture applied to mixed residential and commercial flat buildings. The equally decorated and pronounced front and rear facades illustrate a skilfully embellished outstanding example of Art Deco style brickwork and stucco moulding details. The facades feature creative use of various brick bonds, polychromatic bricks, moulded and embossed stucco in geometric and floral patterns with a central skyline element, and a central decorative arch defining the rear entrance. The building makes significant contribution to the streetscapes of Belmore Road and Arthur Lane. It also has historical significance as a later part of the Inter-War residential development boom in the Randwick municipality and the demand for apartment style housing⁵¹.

Specific Policies

The building should be retained and conserved.

A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes. When opportunity arises the poorly installed flashing at the rear façade should be cut back to minimise its detracting nature. Graffiti removal should be managed in a timely manner to prevent damage to the brickwork.

⁵¹ 'Three-storey Commercial Building', State Heritage Inventory (SHI) Form, Heritage Item ID 2310097, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310097>

Three-storey Art Deco Commercial Building (Item no. I294)

35-43 Belmore Road



Statement of Significance

Built c.1937, the three-storey building at 35-43 Belmore Road is of historical significance as part of the Inter-War period development in Randwick illustrating the demand for apartment style accommodation in response to the rapid increase in population at the time. Demonstrating the key characteristic elements of the Art Deco style, the building is aesthetically significant due to its impressive and distinct brickwork detailing created by combination of geometric pattern, and polychromatic and textured face brick. It combines stretcher bond, header bond, herringbone, shingles, and chevron and stepped piers brickwork throughout the five distinct bays of the facade. The decorative timber joinery and façade fenestration add significantly to the aesthetic quality and architectural integrity of the building complimenting the varying pattern of the brickwork through combination of vertically proportioned mullions and Art Deco glazing⁵².

Specific Policies

The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication Photographic Recording Of Heritage Items Using Film or Digital Capture, should be undertaken before major changes.

⁵² 'Three-storey Art Deco Commercial Building', State Heritage Inventory (SHI) Form, Heritage Item ID 2310095, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310095>

Residential / Commercial Building (Item no. I295)

48-60 Belmore Road



Source: Google Street View (November 2022).

Statement of Significance

Outstanding example of art-deco style building, designed for mixed residential-commercial use. Excellent streetscape contribution.

This site is an outstanding example a building in the Inter-War Art Deco architectural style. The extensively decorated and embellished polychromatic brickwork throughout building's façade show a highly skilled brick workmanship and perfect application of the key characteristics of the Art Deco architectural style⁵³.

Specific Policies -

Not Available on the item's SHI form

⁵³ 'Residential / Commercial Building', State Heritage Inventory (SHI) Form, Heritage Item ID 2310096, accessed [online]
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310096>

Semi-detached pair (Item no. I316)

17-19 Clara Street



Source: Google Street View (November 2022).

Statement of Significance

17 and 19 Clara Street, Randwick are examples of late Federation semi-detached cottages in the Randwick area. The dwellings are of aesthetic value as examples of substantially intact Federation semi-detached dwellings. The setting of the semi-detached cottages has been compromised by surrounding development, particularly the Coles Supermarket development adjacent.

Constructed c1924, 17 and 19 Clara Street are excellent examples of Inter-War semi-detached cottages with influence of late Federation/Edwardian style illustrating the development and subdivision pattern in Randwick at the time. The cottages are substantially intact owing to their reinstated verandas featuring centrally located half-timbered roughcast gable, decorative timber veranda and joinery, and sandstone base. This semi-detached pair stands out within the streetscape of Clara Street, which is dominated by a shopping centre and apartments⁵⁴.

Specific Policies

It is recommended that the properties remain local heritage items on the LEP Heritage Schedule. It is recommended that the original and early fabric, including the front fences, be retained and that the face brick is not to be rendered.

3.5. Heritage Conservation Areas

The following section provides a breakdown of the heritage conservation areas within and adjoining the subject area, identifying their significance, development potential and high-level site specific policies which should guide any future development related to these items. The specific policies do not consider the specifics of the current Randwick Junction Town Centre Strategy (RJTCs) for an item. The proposed strategy is discussed in *Section 4*. The statements of significance for these heritage conservation areas have been extracted from the individual State Heritage Inventory (SHI) form for the sites⁵⁵.

⁵⁴ 'Semi-detached pair', State Heritage Inventory (SHI) Form, Heritage Item ID 2310149, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310149>

⁵⁵ Heritage NSW, State Heritage Inventory database, accessed [online] https://www.hms.heritage.nsw.gov.au/App/Item/SearchHeritageItems?_ga

Randwick Junction Heritage Conservation Area [C15]



Statement of Significance

The Randwick Junction Conservation Area is the only conservation area within the City of Randwick that is focused on a commercial centre. It retains a coherent streetscape character of nineteenth and early twentieth century buildings. Within the conservation area there are two distinct groupings of commercial buildings. These are Belmore Road and the Coach and Horses grouping (centred on the intersection of Alison Road and Avoca Street)⁵⁶.

Development Potential

Specific Policies

D3 Randwick Junction Centre⁵⁷

8 Site Planning

- Provide ground floor retail and/or commercial floor space along all business zoned street frontages, other than the frontage required for access.
- In addition to the continuous commercial street frontage, the ground floor commercial area should have a minimum depth of 10 metres in order to achieve a viable shop or office size (except in heritage situations where it may be different).
- Development should relate to the dimensions and shape of the site.
- Development should integrate with the surrounding area through consideration of streetscape and landscape design and pedestrian and cycle links.
- Retain and integrate heritage items or contributory buildings and significant features such as stone fencing and retaining walls.
- Development must minimise impacts on the amenity of neighbouring sites.

9 Building envelopes

- The FSR and building height controls set by RLEP together with DCP envelope controls define the overall built form and scale of development.
- New development should be built to the street alignment and to the side boundaries of the allotment.
- Where adjoining and nearby development is set back from the street, new development should be consistent with the setbacks of adjoining development or the dominant setbacks along the street.
- Where buildings are setback from the front boundary, such as the school and terraces along Avoca Street, fences are to be used to reinforce the street alignment and provide a strong visual transition point between public and private space.

⁵⁶ 'Randwick Junction Heritage Conservation Area', State Heritage Inventory (SHI) Form, Heritage Item ID 2310526, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310526>

⁵⁷ Randwick Comprehensive Development Control Plan (2013), 'D3 Randwick Junction Centre', Randwick City Council, pp. 10-11, accessed [online] https://www.randwick.nsw.gov.au/_data/assets/pdf_file/0017/13742/Randwick-Junction-Centre.pdf

High Cross Heritage Conservation Area [C12]



Source: Google Street View (November 2022).

Statement of Significance

The High Cross Conservation Area consists of High Cross Park, urban areas to the north-east and south, and part of the Prince of Wales Hospital to the west.⁵⁸

Development Potential

Specific Policies

4.6 High Cross Heritage Conservation Area⁵⁹

4.6.4 Guidelines for Change

Alterations & Additions

Rear additions should not be prominent in the streetscape nor comprise the integrity of the original roof. Additions to terraced buildings should not compromise the integrity of relatively intact rear wings and should be consistent with the scale and form of surrounding rear wings.

Carparking

Where driveway access along the side of the dwelling was available, garages were traditionally provided in the rear yard of the dwelling, and this remains the preferred location. Otherwise an open carport can be provided to the side of the dwelling, set back from the front wall of the dwelling.

3.6. Potential Heritage Items

Following are the potential heritage items proposed for heritage listing as part of the Randwick Council Comprehensive Planning Proposal LEP Review lodged with the Department of Planning in September 2022.

1 Belmore Road (Above awning Facade and Awning only)

Statement of Significance

The façade of 1 Belmore Road is architecturally distinctive due to its Inter-War Art Deco style which strongly contributes to the streetscapes of Belmore and Alison Roads. The building presents a rare example of a commercial style Inter-War Art Deco building in the

⁵⁸ 'High Cross Heritage Conservation Area', State Heritage Inventory (SHI) Form, Heritage Item ID 2310523, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310523>

⁵⁹ Randwick Comprehensive Development Control Plan (2013), 'B2 Heritage - 4.6 High Cross Heritage Conservation Area', Randwick City Council, pp. 48, accessed [online] https://www.randwick.nsw.gov.au/_data/assets/pdf_file/0003/13737/Part-B-General-Controls.pdf

1 Belmore Road (Above awning Facade and Awning only)



Randwick area as most buildings from this period are associated with residential flats. It features characteristic elements of the Art Déco style including polychromatic face brick facades with dominant red bricks, herringbone wall panels, vertically emphasised two storey high engaged piers with decorative projecting parapets⁶⁰.

Specific Policies

- It is recommended that only the above awning facade and awning are to be listed on Schedule 5 of the Randwick LEP 2012. It is further recommended that the upper level signage on the splayed corner be removed as it obscures much of the significant fabric.
- A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication *Photographic Recording Of Heritage Items Using Film or Digital Capture*, should be undertaken before major changes.

"Warrington"

167-171 Alison Road, Randwick



Statement of Significance

This Victorian Filigree style terrace at 167-171 Alison Road, Randwick is an architecturally distinctive mixed use residential and commercial building that contributes to the streetscape of Alison Road through its free classical characteristic detailing of the parapet and wrought iron filigree balustrades with matching frieze and fringe. The building once formed part of a larger corner building constructed by the Hebblewhite family in 1887 on one of the earliest land grants in Randwick. The original building extended to 1 Belmore Road and was partially demolished in the 1920s leaving the subject building as a remnant example of the Victorian era development boom in Randwick. The building is evidence of the development pressures occurring in Randwick Junction during the Inter-War period⁶¹.

Specific Policies

- Listing of the building on Schedule 5 of the Randwick LEP 2012 as a Heritage Item is recommended.

⁶⁰ Draft Inventory Form prepared by City Plan Heritage (June 2015).

⁶¹ Draft Inventory Form prepared by City Plan Heritage (June 2015).

"Warrington"

167-171 Alison Road, Randwick

- Full interior inspection should be undertaken on this building to establish if any significant original elements dating from the 1887 construction remain. An inspection of the ornate parapet is further recommended to detect and repair any deterioration of the face brick and sandstone.
- A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication *Photographic Recording Of Heritage Items Using Film or Digital Capture*, should be undertaken before major changes.

"Montrose"

179-181 Alison Road, Randwick



Statement of Significance

Constructed in 1888 this two storey Victorian Italianate building is a remnant example of the nineteenth century development boom in Randwick. It is known as 'Montrose' and is associated with one of the early settlers of Randwick, Samuel Hebblewhite. Although it has been modified through addition of a commercial tenancy, the terrace remains largely intact and is an architecturally distinctive element within the streetscape of Alison Road featuring the key characteristics of its style that was once immensely popular in Australia as a domestic style and influenced the rapidly expanding suburbs of the 1870s-1880s⁶².

Specific Policies

- It is recommended that the building be listed as a Heritage Item on Schedule 5 of the Randwick LEP 2012.
- The amount of advertising signage from the bottle shop should be reduced as it detracts from the significant Victorian Italianate presentation of the building.
- The building should be retained and conserved. A Heritage Assessment or Heritage Impact Statement should be prepared for the building prior to any major works being undertaken. Archival photographic recording, in accordance with the Heritage Office publication *Photographic Recording Of Heritage*

⁶² Draft Inventory Form prepared by City Plan Heritage (June 2015).

"Montrose"

179-181 Alison Road, Randwick

Items Using Film or Digital Capture, should be undertaken before major changes.

25 Waratah Avenue, Randwick (To be include within the curtilage of Heritage Item I295 - 48-60 Belmore Road)



Source: Google Street View (May 2023).

Existing Statement of Significance for 48-60 Belmore Road (item no. I295)

Outstanding example of art-deco style building, designed for mixed residential-commercial use. Excellent streetscape contribution.

This site is an outstanding example a building in the Inter-War Art Deco architectural style. The extensively decorated and embellished polychromatic brickwork throughout building's façade show a highly skilled brick workmanship and perfect application of the key characteristics of the Art Deco architectural style⁶³.

Specific Policies -

Not Available on the existing item's SHI form

⁶³ 'Residential / Commercial Building', State Heritage Inventory (SHI) Form, Heritage Item ID 2310096, accessed [online]
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310096>

4. SIGNIFICANCE OF RANDWICK JUNCTION TOWN CENTRE

Following is the statement of significance with key recommendations extracted from Heritage Conservation Area Review Randwick Junction - Belmore Road and Avoca Streets, prepared by City Plan Heritage (September 2015)

A largely intact traditional commercial centre with many good examples of buildings from the Victorian, Federation and Inter-war period.

The Randwick Junction Heritage Conservation Area is centred on the Randwick Junction commercial centre. It is generally bounded by Belmore Road, Alison Road and Avoca Street, Randwick.

The Randwick Junction Conservation Area is the only conservation area within the City of Randwick that is focused on a commercial centre. It retains a coherent streetscape character of nineteenth and early twentieth century buildings. Within the conservation area there are two distinct groupings of commercial buildings. These are Belmore Road and the "Coach and Horses" grouping (centred on the intersection of Alison Road and Avoca Street)⁶⁴.

Key Recommendations⁶⁵

- *Location of significant stone kerbs and gutters should be noted - Significant stone kerbs and gutters are evident at Bell Lane. These are considered contributory elements. The kerbs and gutters should be retained and conserved and appropriate policies incorporated into the DCP.*
- *A co-ordinated colour scheme should be implemented - Randwick City Council currently has excellent guidelines regarding colour schemes in place for the Randwick Conservation Area (Annexed at Appendix 3). It is noted that 'original colour schemes' are referenced Randwick Comprehensive Development Control Plan (DCP) 2013, however more specific guidelines should be established in the form of Streetscape Study similar to that of Newtown King Street and Enmore Road Paint Scheme by City of Sydney. This will enhance the consistency and enforce a good heritage practice within the Randwick Junction HCA. Existing and future guidelines should be retained and enforced as conditions of consent for any future Development Applications, particularly in the case for the following properties:*
 - *153-155 Alison Road, Randwick*
 - *104 Belmore Road, Randwick*
 - *131 Belmore Road, Randwick*
 - *133 Belmore Road, Randwick*
 - *11-15 Belmore Road, Randwick*
 - *140 Avoca Street, Randwick*
 - *117 Avoca Street, Randwick*
- *Awning guidelines are currently contained in Part D3 of the DCP (Randwick Junction Centre). However, they do not include guidelines or controls for original awnings. It would be beneficial for these guidelines and the additional awning guidelines identified in Section 9.2.6 of the DCP. They should be incorporated into Part B2 to reinforce retention, maintenance and reinstatement of traditional and original awnings where documentary and physical evidence exist.*
- *A co-ordinated signage strategy should be incorporated into the DCP - A signage strategy for the Randwick Junction HCA should be prepared to prevent further visual cluttering and physical damage to the significant facades of the buildings. Controls for signage are not clearly defined either in Part*

⁶⁴ City Plan Heritage (September 2015), 'Established Statement of Significance - Heritage Conservation Area Review Randwick Junction - Belmore Road and Avoca Streets', Randwick City Council, pp 70.

⁶⁵ City Plan Heritage (September 2015), 'Conclusion and Key recommendations - Heritage Conservation Area Review Randwick Junction - Belmore Road and Avoca Streets', Randwick City Council, pp 78.

B2 or Part 2 of the DCP. The guidelines outlined in Section 9.2.7 should be included in Part B2 and/or Part 2 of the DCP until a comprehensive signage strategy implemented as part of the planning controls.

- *Rezoning of any areas within the Randwick Junction HCA is not recommended in order to maintain the integrity of existing uses and, in the case of 68 High Street [sic. 166-168 Belmore Road], potentially reinstate former residential use. No increase to height controls is considered appropriate.*
- *It is noted that the Randwick Junction Town Centre Strategy permits additional height (up to 10 storeys) and density on this key 'gateway' site that adjoins the Light Rail stop. The strategy further aims to encourage commercial, health and medical uses in the podiums with residential apartments above.*
- *It is recommended that a policy/control be included in the DCP (Part D3) for rectification of intrusive changes to the facades of contributing buildings within the Area. This could be encouraged as part of Development Applications where opportunity arises.*

5. THE PROPOSED PLANNING STRATEGY

Randwick Junction Town Centre is a retail and commercial centre that will be zoned primarily E2 Commercial Centre under the NSW Government Employment Zones Reform implemented as part of the Randwick LEP Review and is located at the terminus of the CBD and Southeast Light Rail route. Randwick Hospital Campus and the University of NSW Kensington Campus are located to the south of the town centre. Considering the context and location of the subject area with its significant employment component, the strategy sets out the need for housing and convenient retail areas (to enhance the economy) with landscaped open spaces for the community and improved active and public transport connectivity. The Planning Strategy sets out the vision, strategies and implementation actions to guide the sustainable growth and physical development of the Randwick Junction Town Centre (RJTC) over the next 15 years, with improvements to the public domain to enhance the liveability, sustainability and economic prosperity of the town centre.

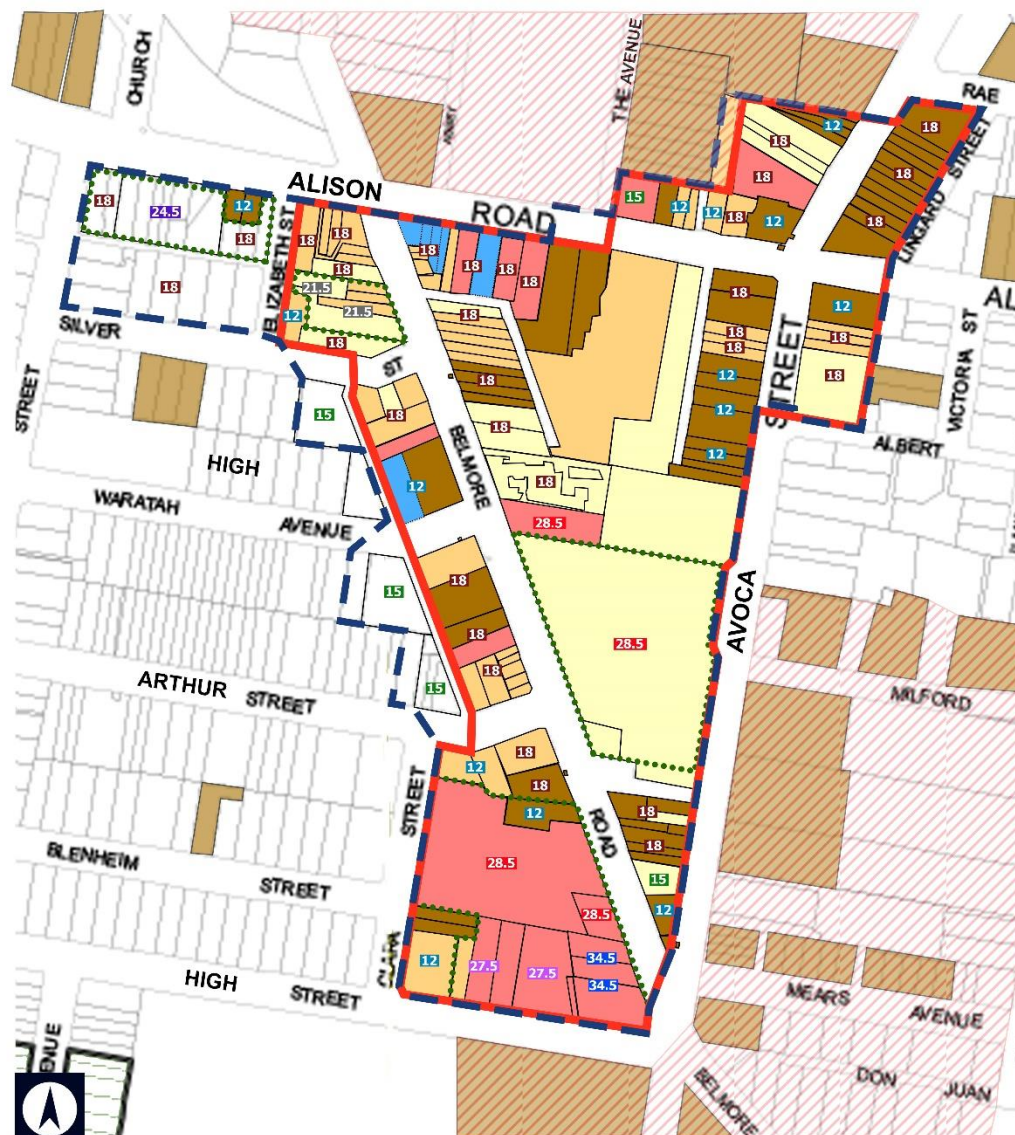
The number of jobs in Randwick Junction Town Centre are projected to grow. Post Covid, the growth rate has slowed, none-the-less employment in the LGA is projected to grow by 18,122 jobs between 2021 and 2036 (Randwick Economic Study Update 2021, SGS Economics and Planning, August 2021). Growth is expected in the industries of retail trade, food and beverages, education and healthcare. The purpose of the proposed town centre is to achieve the projected moderate jobs growth and provide the appropriate type of employment floor space within the town centre. Its proximity to the Randwick Education and Health Strategic Centre will support the employment needs of an emerging innovation district which will stimulate economic activity and jobs growth.

The proposal for the town centre has identified four strategic sites that have redevelopment potential and are strategically located (Refer to the Strategic Sites map). The Strategic Sites leverage their proximity to the Light Rail station in the south of the town centre and also encourage the redevelopment of the sites to stimulate economic activity in the north and northeast of the centre.

Randwick Junction is well served by public transport with access to bus services and light rail. There will be some interchange between bus stops on Belmore Road and the light rail stops on High Street.

The town centre is located within the Randwick Junction Heritage Conservation Area that contains a mixture of historical building stock, neutral and detracting development. It was the first suburban municipality in NSW commencing in 1859, with a centre for commercial activity. The site has a number of historic buildings and contributory items of significance from Federation and Inter War periods that were built since the establishment of the commercial village in the mid nineteenth century.

The following page provides a map of the study area and identifies the heritage significance ranking and proposed building heights, and the strategic heights.



LEGEND			
	Randwick Junction HCA		Contributing Item
	Randwick Junction Boundary		Neutral Item
	Heritage Item - General		Detracting Item
	Heritage Item - General (outside the site boundary)		HCA's in the vicinity
	Proposed Heritage Item - General		Lots outside Randwick Junction HCA
	Key Strategic Site		

RANDWICK JUNCTION TOWN CENTRE

June 2023



Randwick City Council
a sense of community

Figure 23: Randwick Junction Town Centre Strategy with strategic sites overlayed on heritage and contributory properties (Source: City Plan Heritage (2023)).

5.1. Three Key Development Scenarios

The following three potential development scenarios have been identified within the town centre⁶⁶:

- *Mid-rise mixed-use buildings in strategic sites*
- *New infill transitional buildings in valued heritage context that includes heritage items and contributory buildings fabric*
- *Incremental alterations and additions of heritage listed and contributory buildings (generally no change to the current controls)*

The strategic sites are identified on Figure 23.

5.1.1. Mid-rise mixed-use buildings in strategic sites

Four sites have been identified in the town centre as strategic sites:

- (1) *High Street Strategic Site which is bounded by High Street on the south side; Belmore Road to the east; Clara Street to the west and Arthur Street and June Moore Place to the north - Proposed maximum building height of 10 storeys (approximately 34m).*
- (2) *Royal Randwick Shopping Centre Strategic Site is bounded by Short Street to the south; Belmore Road to the west; Avoca Street to the east and Marcellin College to the north - Proposed maximum building height of 7 storeys (equivalent to an 8 storeys residential building).*
- (3) *Randwick Club Strategic Site, which is bounded by Alison Road to the north; Botany Street to the west; Elizabeth Lane to the south and Elizabeth Street to the east. This block primarily contains the Randwick Club and existing residential properties along Alison Road up to Botany Road - Proposed maximum building height is 7 storeys (approximately 24.5m).*
- (4) *Former CBA Bank Strategic Site is bound by Belmore Road to the east and in part by Elizabeth Street to the west - Proposed maximum building height is 6 storeys (approximately 21.5m).*

5.1.2. New Infill Development in Valued Heritage Context

This strategy recommends a street wall height limit that aligns with existing heritage and contributory buildings (generally two or three storeys in height) and no greater than 12m on blocks that have no heritage or contributory buildings, along the main thoroughfares of Belmore Road, Alison Road and Avoca Street with a minimum upper level setback of 4m for new infill development adjacent to heritage/contributory item(s).

A central component of up to 5 storeys (approximately 18m) may be permitted where this element is adequately setback into the development site and the scale is therefore mitigated when viewed from the relevant main street front level.

New development (including additions) must reinforce the predominant two or three storey street wall, with level four and above setting back four metres from street wall, which applies to the study area.

5.1.3. Incremental modifications - heritage/contributory buildings

To generate additional residential/commercial capacity, these changes include redevelopment of heritage or contributory buildings within the town centre that could be modified through small-scale additions/extensions. The strategy requires landowners to preserve and restore the valued heritage character of these buildings.

⁶⁶ Randwick Junction Town Centre Strategy (August 2020), '8.3 - Built Form Strategy', Randwick City Council, pp. 35.

This strategy recommends a street wall height limit of two or three stories along the main thoroughfares of Belmore Road, Alison Road and Avoca Street to match existing heritage and contributory buildings with any additional storeys to be setback by a minimum of 4m.

Rear extensions and laneway development with ground floor retail uses (Arthur Lane and Bell Lane in particular) are strongly encouraged to promote future activation of these laneways.

6. OPPORTUNITIES AND CONSTRAINTS

Due to the extant history and established heritage significance of the Randwick Junction Town Centre study area there are a number of associated constraints related to heritage that will impact development going forward.

6.1. Constraints

As detailed in Section 1.4 (Heritage Listing), the Randwick Junction Town Centre (RJTC) contains 29 heritage items and the Randwick Junction heritage conservation area listed in Part 1 and Part 2 of Schedule 5 of the Randwick LEP 2012 respectively, and a number of listed items with connected archaeological potential. The identified significance of these sites warrants protection and conservation for future generations, and to maintain the integrity and character of the conservation area as part of the environmental heritage of Randwick LGA. Any proposed changes to these items will have to be undertaken in conjunction with the existing statutory protections that are associated with local planning controls and heritage policy. The assessed significance of the RJTC area raises obligations in relation to the appropriate heritage listings of sites, their recommended management and relevant statutory controls within the area.

The traditional lot size and rear accessways of the RJTC reflect the history of use, having a mixture of commercial, civic, healthcare, and residential uses. The narrow width of lots throughout the proposed area, primarily along Belmore Road (retail spaces) and Avoca Street pose challenges for development in the Town Centre. It is considered that this 'fine-grained' development pattern provides the small-scale village character of the town centre, and that any redevelopment will need to preserve this character and scale.

The triangular shape of the Randwick Junction Town Centre is defined by three major streets: Belmore Road, Avoca Street and Alison Road. Major corner sites exist at these intersections that are prominent and crucial features of the heritage character of the area, such as the 1874 Statue of Captain Cook (first statue erected in Australia⁶⁷) at the corner Belmore Road & Avoca Street. This results in the need to maintain particular focus on the built prominence of these sites and will restrict surrounding development to areas of least detracting of view corridors to and from these sites and their skyline setting and backdrop. The resulting constraint will require an appropriate setback on both streets associated with the corner site.

Another significant constraint associated with these prominent heritage sites are the traditional roof forms. Some of the heritage items within RJTC have traditional sloped roofs with elements such as chimneys, ridge lines and party walls. These building features and elements should be conserved and appropriate setbacks to new building elements or to additional storeys provided in proportion to the existing building height.

The historic low-rise nature of the study area is characterised by the consistent 2-3-storey façade height of the majority of the buildings along the main streets in the form of street walls defining the heritage streetscape. This is another crucial element of RJTC that must be conserved; any proposed new development is required to have a podium level in line with the existing prominent street wall height of respective streets and constrained by the necessity to be both consistent with and respectful to the existing height and playfulness of parapets (some with distinctive decorative skyline accents and sculptural elements) of the heritage items within the area. It is considered necessary that podium development within the town centre should not exceed the existing parapet height of the adjoining heritage items or contributory buildings. This will help to ensure the consistency and appreciation of the significant facades within the town centre are not compromised.

Further, the construction of large-scale towers within the strategic sites of the town centre has the potential to result in the loss of views to the sky and to generate significant overshadowing which may impact the fabric of both heritage items and the general character of the town centre. Development which will result in

⁶⁷ 'Statue of Captain James Cook', State Heritage Inventory (SHI) Form, Heritage Item ID 2310094, accessed [online] <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2310094>

large scale changes to natural illumination of the town centre public domain and residences will require careful consideration to determine the impact on the unique streetscapes of the town centre.

Due to the unique historical configuration of the lots within the town centre and their influence on the significant character of the area, it is necessary to undertake the preparation of a Conservation Management Plan (CMP) or Conservation Management Strategy (CMS) for relevant sites whenever the amalgamation of existing lots is proposed.

6.2. Opportunities

Changes to the town centre that engage heritage conservation as a guiding and integral principle of change could provide the opportunity to put conservation as a paramount consideration of new developments and create opportunities for the restoration and renewal of heritage buildings and items, and for designed heritage interpretation. Designed heritage interpretation (such as interpretation by fabric) relevant to Randwick Junction's history as a commercial, suburban area could result in a positive impact on the visual amenity of the sites and the way people engage with the area, both locals and visitors. The opportunity to engage in heritage interpretation within the development process would help to strengthen the culture of RJTC and solidify its identity. This opportunity is critical in the future development in shaping both the continuation of the town centre's cultural identity and improvement in wider economic value. This potential opportunity to apply meaningful interpretation could be a key element in the consolidation of Randwick's historic identity amongst the transitional future of the RJTC. There should be provision for a comprehensive heritage interpretation strategy which follows a coordinated approach to interpretation within the town centre.

Physical opportunities that arise from the RJTC include the opportunity to create a consistent plan with regard to upper level setbacks. Adhering to a consistent setback would assist in the creation of a more amenable approach to development which manages the visual impact of development on the significant heritage fabric of the town centre.

Works related to the development of the RJTC open up significant opportunities to engage in heritage conservation works within the area. The abundance of significant heritage items and aesthetic features that characterise the area has been subject to a number of intrusive changes to their fabric and form throughout recent history. If these conservation issues were to be rectified through work related to development, this could potentially improve the appreciation of their significance, the aesthetic of the streetscape and the future economic value of the subject RJTC. It should be a significant part of any ongoing Planning Proposal/Strategy for RJTC that the development to be undertaken on the major streets such as Belmore Road, Avoca Street, and Alison Road should be utilised to assist in the revitalisation of the town centre. This could be realised in the restoration of original paint schemes, removal of detracting signage or service installations, retention of existing prominent street walls or repair of original fixtures, among others.

The opportunity for celebration of the rich Aboriginal cultural heritage of the Gadigal Nation should also be taken as part of the new developments. Heritage interpretation should not only be limited to European heritage but also integrate cultural heritage into the design and public art.

There is also an opportunity to reduce clutter along Belmore Road and organise the movement within the town centre by developing active transport strategies to improve the connectivity for the community within and outside the RJTC. The transport strategies could further focus on enhancing the walking and cycling catchments within the town centre.

Randwick is one of the first suburban municipalities in NSW since 1859 and has always been a centre for commercial activity. The early commercial hub has a traditional commercial "strip" along Belmore Road that has a series of heritage items and items of contributory value, street wall with many buildings with original face brickwork and heritage colour schemes. Another opportunity includes restoring the fabric and reinstating an appropriate heritage colour scheme to the heritage and contributory buildings along the main

streets, restoring the contributory values of buildings within the town centre that have been modified, damaged or that have deteriorated over time. This could be achieved by preparing a Signage Strategy and specific streetscape study to establish appropriate colour schemes and identify the building elements that could be reinstated to improve and enhance their contribution and remove uncharacteristic façade elements that detract from the heritage qualities of the town centre.

7. ANALYSIS OF VISION FOR RANDWICK JUNCTION TOWN CENTRE

The proposed Planning Strategy for Randwick Junction Town Centre (August 2022) includes changes to the broader planning framework with the objective of accommodating moderate growth, within the overall vision of the town centre becoming an active, accessible and attractive place to live, work and relax. The RJTC will be confident, engaging and vibrant and promote commercial and retail growth with a balanced approach to transport and traffic, while protecting and enhancing the human scale streetscape and the heritage significance of the area by promoting excellence in architecture, which positively contributes to the public realm.

The RJTC has heritage items listed at the local government level and listed on the State Heritage Register that preserve the unique character of the area. Historically, RJTC is a low to medium rise, low density collection of commercial, retail and residential buildings. The study area is representative of the historic development of the town centre and includes heritage items spread over a series of small and medium sized lots. The fine grain of the lots in the town centre should be preserved when redevelopment occurs, expressed in the modulation and articulation of the streetscape frontages.

The study area has three prominent roads (Belmore Road, Avoca Street and Alison Road) and retains the character of a historic streetscape despite recent development of a few sites. The study area is highly significant and has a collection of historic low scaled buildings. These characteristics provide the potential to reinforce this distinctiveness, while also becoming a major commercial and residential centre. The study area has been divided into Sub Areas as illustrated below and in the following pages.

Key Map of Sub Areas of the Randwick Junction Town Centre

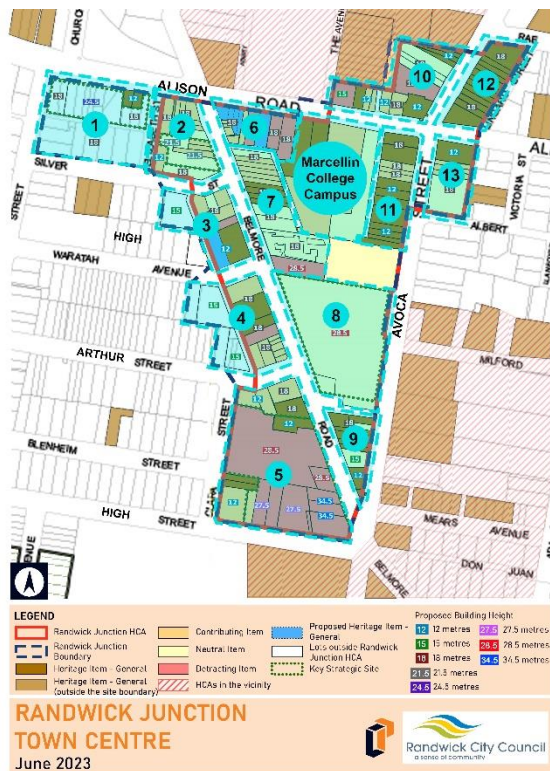
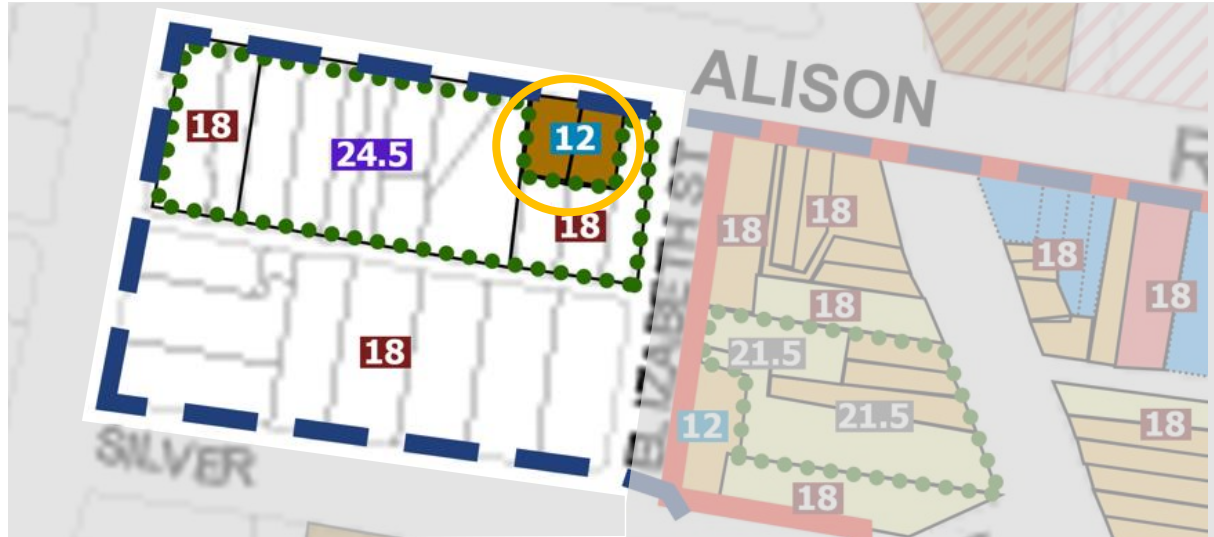


Figure 24: Key Map for Randwick Junction Town Centre Study Area with sub areas 1 to 13 (Source: City Plan Heritage).

Sub Area 1 (Strategic Site)



Key Findings & Recommendations

- The terraces located on Alison Road (heritage items I255 and I256 - circled in yellow) should not be developed to 12 metres.
- The proposed 24.5m height for the strategic site should be in the form of a setback/podium with tower or two-tower configuration to reduce the impact on the streetscape when viewed from Alison Road or Belmore Road.
- The proposed development should avoid bulky and continuous block development and retain the original narrow subdivision pattern and rhythm of the streetscape with new mixed-use development modulated in a similar manner as the original residential flat buildings and/or terraces.
- Design of the new developments (within the boundary of the strategic site) should strive for design excellence, innovation and creativity and relate to each other and should be guided by a comprehensive heritage assessment in order to guide the proposed development.
- The 'Specific Policies and Recommendations' (Section 3.4) associated with the heritage items in this sub area should be taken into consideration as a part of the proposed development.

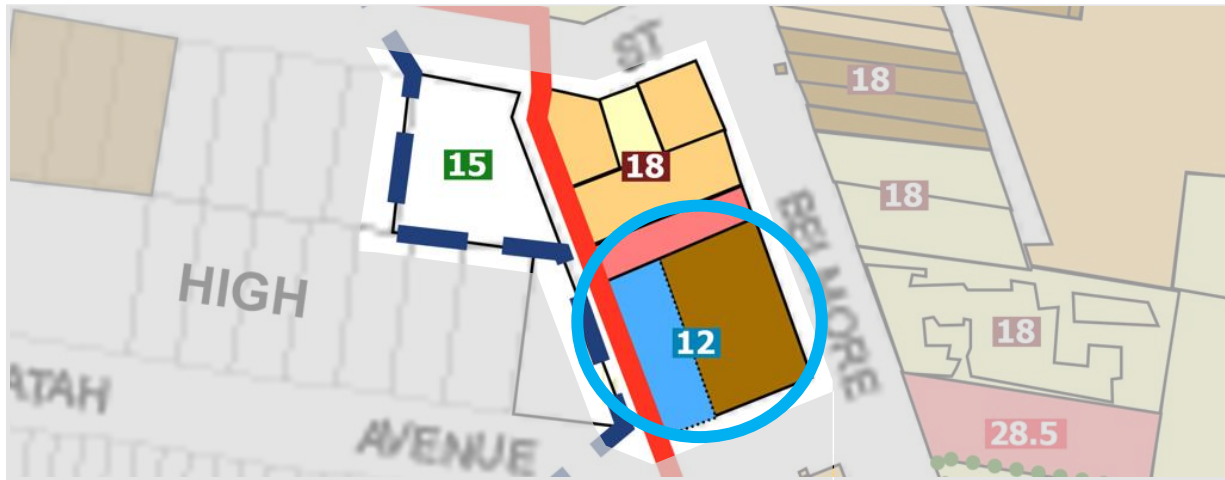
Sub Area 2 (Key Strategic Site)



Key Findings & Recommendations

- The maximum height proposed for contributory and neutral items, located at the corner of Belmore Road and Alison Road are acceptable from a heritage perspective.
- The 21.5m height proposed for the strategic site should retain and strengthen the existing two storey predominant street wall height, and then setback at the upper level behind the street wall parapet.
- The proposed development should avoid bulky and continuous block development, retain the narrow subdivision pattern and rhythm of the traditional shopfronts.
- The predominant two storey street wall height established by the contributory building street facades and parapets should be matched in height by any new infill development.
- Design of the new development should strive for design excellence, innovation and creativity and relate to each other and should be guided by a comprehensive heritage assessment or a Conservation Management Strategy or Plan (CMS or CMP) in order to guide the management of established and assessed heritage significance of each item as well as the contributory buildings.

Sub Area 3



Source: Google Street View (accessed February 2023).



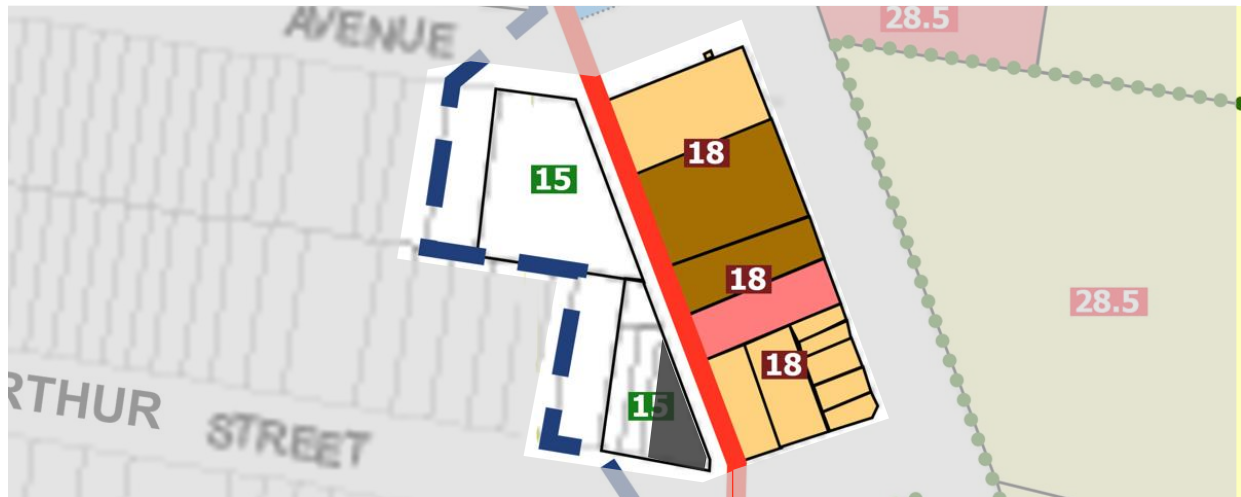
Source: Google Street View (accessed February 2023).

Key Findings & Recommendations

- The heritage items located north of Waratah Avenue, at the corner of Belmore Road and Waratah Avenue (indicated in blue circle) should not be over developed as it only has the potential to have maximum one-storey behind the parapet. The heritage items have rich, Inter-War façade detailing that should be retained as a part of the development.
- The development along the west side of Arthur Lane and Waratah Avenue, proposed for 15m height is acceptable from heritage perspective as the allotments and comprised of contemporary development with no significance.

- The contributory and heritage items (with flat roofs or terraces) along west of Belmore Road and adjacent to Silver Street proposed for 18m height upgrade is acceptable from heritage perspective and the proposed development should be well setback (minimum 4m) from the existing building line.
- If the detracting item located west of Belmore Road, adjacent to the facebrick residential flat building is proposed for redevelopment, the new development should have at least neutral characteristics and detailing and should not detract from the street presentation.
- The existing building alignment should be maintained, and the proposed new development above should be well setback from the existing building line.
- The proposed development should avoid bulky and continuous block development to retain the existing subdivision pattern and rhythm of the traditional shopfronts.
- The predominant two and three storey street wall height established by the heritage and contributory building facades should be matched in height by any new infill development.
- The 'Specific Policies and Recommendations' (Section 3.4) associated with the heritage items in this sub area (if applicable) should be taken into consideration as a part of the proposed development.

Sub Area 4



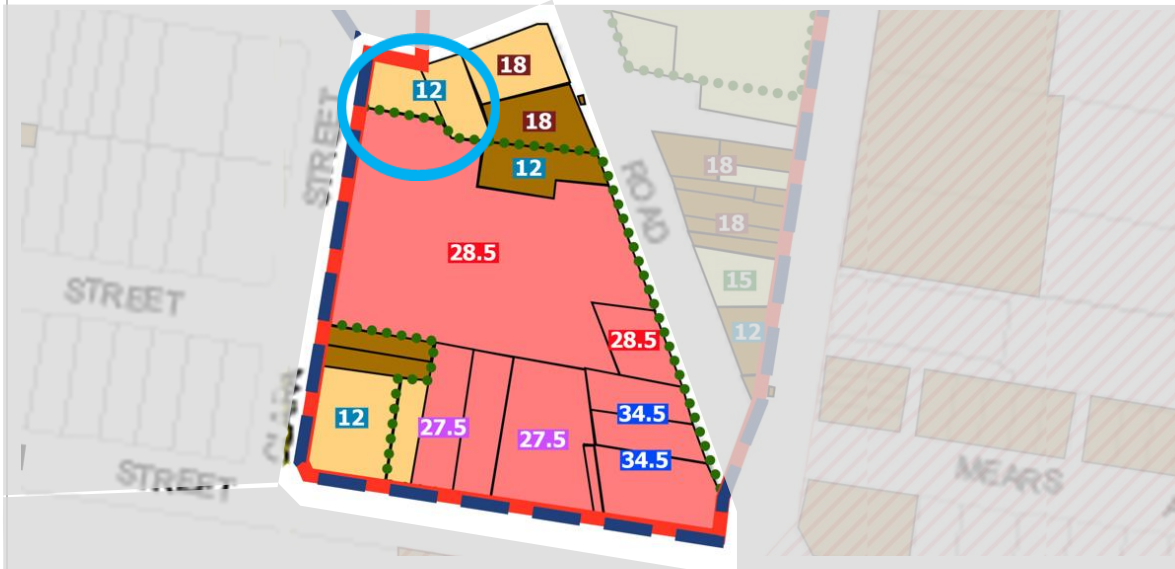
Source: Google Street View (accessed November 2022).

Key Findings & Recommendations

- The houses located north of Arthur Street and west of Arthur Lane are proposed for 15-meter height. The houses are located outside the Randwick Junction Town Centre HCA. Only the house (indicated in grey) at the corner of Arthur Street and Arthur Lane can be replaced and developed as per the proposal. The other two residential terraces at 65 and 67 Arthur Street, located adjacent to it have original detailing and characteristics and their development is not recommended from a heritage perspective regardless of not being heritage listed or located within a heritage conservation area. If development of these residential terraces is proposed the owners should be encouraged to retain the front intact portions and develop the higher new additions behind them in order to maintain the characteristics next to the HCA. The residential flat building south of Waratah Street and west of Arthur Lane is proposed for 15m height, which is outside the HCA, and is acceptable.
- The commercial development along Belmore Road could be developed up to 18 meters, but the original façade detailing, especially the corners should be retained. The proposed new development should be placed beyond the corner banding details at 104 Belmore Road.
- The existing building alignment should be maintained, and the proposed new development should be well setback at the upper level from the existing building line.

- The proposed development should avoid bulky and continuous block development to retain the existing subdivision pattern and rhythm of the traditional shopfronts.
- The predominant two and three storey street wall height established by the heritage and contributory building facades should be matched in height by any new infill development.
- If the detracting item on the west side of Belmore Road, adjacent to the facebrick residential flat building is proposed for redevelopment, the new development should have a high-quality architectural expression and detailing and should contribute to the street presentation.
- The 'Specific Policies and Recommendations' (Section 3.4) associated with the heritage items in this sub area (if applicable) should be taken into consideration as a part of the proposed development.

Sub Area 5 (Key Strategic Site)



Source: Google Street View (accessed February 2023).

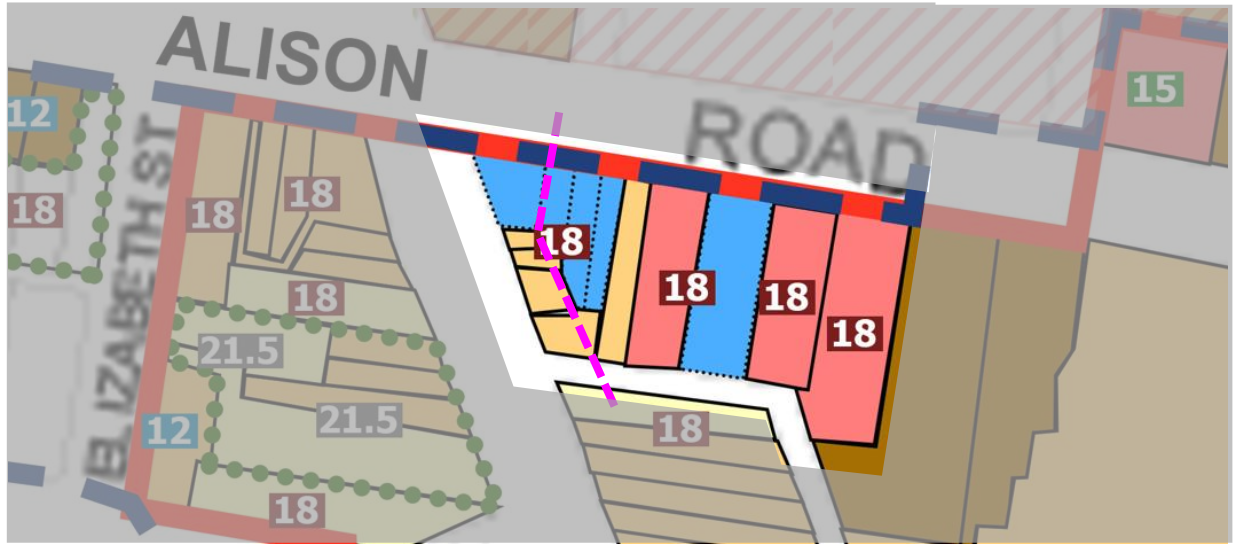
Key Findings & Recommendations

- The contributory items located south of Arthur Street at number 66 (indicated in blue circle) are proposed for development with maximum height of 12m. The existing contributory items are single-storey residences with early double-fronted gabled roof characteristics and detailing and hence, the proposed height upgrade is not considered appropriate from a heritage perspective. Any redevelopment for these contributory items should be consistent with alterations and additions to contributory buildings maintaining their predominant gabled roofscape with rooftop additions placed

at the rear where sufficient land and space is available without overwhelming the existing public domain scale and form of the items.

- The strategic site development would preferably by mid-rise development in stepped form with individual terraces, rather than one bulky high-rise development 28.5 – 34.5m high.
- Avoid continuous block development and retain the existing subdivision pattern and rhythm of the traditional shopfronts, where applicable.
- The development should be setback at the upper levels (above the podium) from both High Street and Belmore Road, considering the impact on low-to-medium density development including heritage and contributing items. The proposed mid-rise tower development should be setback approximately 4m at the upper levels above the podium height, which could be at the height of the existing building at 62 High Street or lower similar to the height of the Contributory buildings at 56-58 High Street. This will allow maintaining a wider skyline along the street when viewed from the west and east approaches thus reducing the overwhelming impact and corridor-like street wall.
- 60 High Street –The subject property has significant characteristics and detailing, including decorative front gables, front facing veranda with timber posts supported on painted brick knee walls, and timber windows with coloured glazing. The significant characteristics should be retained as a part of any future proposal with the development commencing at the rear of the site where later extensions and non-significant fabric are located.
- Design of the new developments (especially within the boundary of key strategic site) should strive for design excellence, innovation and creativity and somehow relate to each other and should be guided by a comprehensive heritage assessment in order to guide the proposed development.
- The predominant two and three storey street wall height established by the heritage and contributory building facades should be matched in height by any new infill development.
- The 'Specific Policies and Recommendations' (Section 3.4) associated with the heritage items in this sub area (if applicable) should be taken into consideration as a part of the proposed development.

Sub Area 6



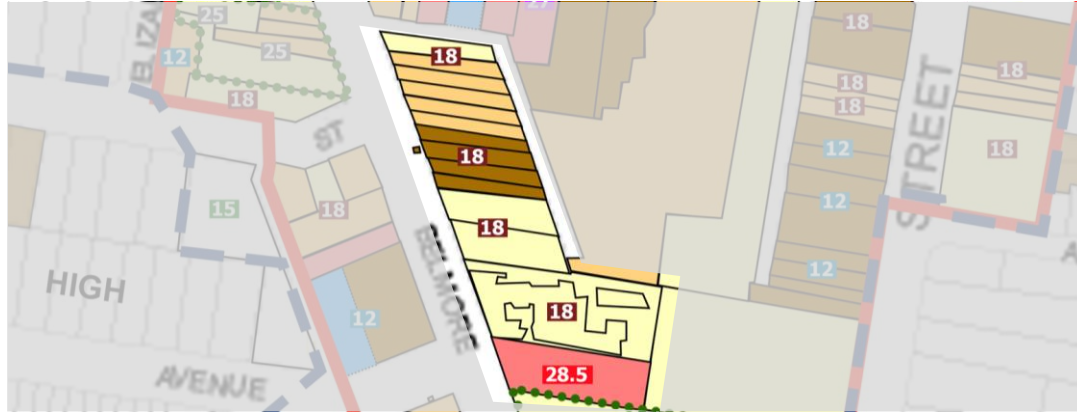
Key Findings & Recommendations

- This area, located south of Alison Road has heritage items and contributory buildings. The maximum height proposed for the west of the area is 18m and for the east is 27m. Detracting commercial development is located at the front of the early Victorian period residences at 183-185 and 187 Alison Road, which are nominated as potential heritage items under the recent Randwick Heritage study Review. There is the potential to incentivise the removal of detracting additions and to restore these original two storey grand houses. Additional height could be permitted at the rear

of the property setback from the street frontages, to restore the original Victorian streetscape character.

- The proposed building height (18m) could be achieved for the west of the subject area; however, it should be setback at the upper level from the building line.
- The corner heritage item, located at the south-eastern corner of Alison Road and Belmore Road should not undergo height upgrades (indicated in pink). Any additional storeys setback from the parapet of the 3-15 Belmore Road properties.
- The predominant two and three storey street wall height established by the heritage and contributory building facades should be matched in height by any new infill development.
- The 'Specific Policies and Recommendations' (Section 3.4) associated with the heritage items in this sub area (if applicable) should be taken into consideration as a part of the proposed development.

Sub Area 7



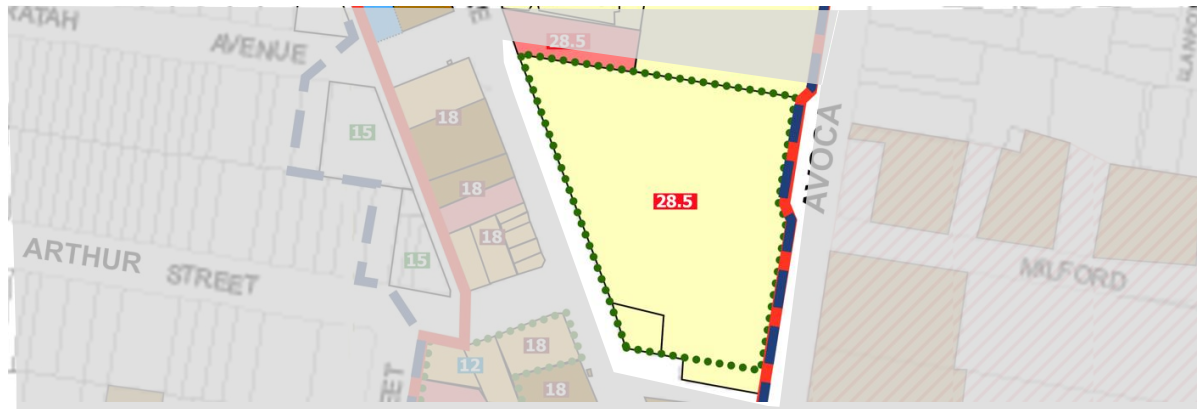
Source: Google Street View (accessed February 2023).



Key Findings & Recommendations

- Generally, the contributory and heritage items (with flat roofs or terraces) located on the east side of Belmore Road are proposed to be 18m in height and this is acceptable from the heritage perspective. The proposed development envelopes should be well setback (minimum 4m) from the existing building street frontage.
- The only exception is the existing eight storey hotel building at 65-71 Belmore Road that is proposed to be 28.5m on the HOB map.
- The maximum height proposed for heritage, contributory and neutral items, located along the east side of Belmore Road are acceptable from a heritage perspective. If the detracting items are proposed for redevelopment, the new development should make a positive contribution to the architectural quality and detailing and to the streetscape.
- The existing building alignment should be maintained, and the proposed new development should be setback at the upper level from the existing building line.
- The proposed development should avoid bulky and continuous block development to retain narrow subdivision pattern and rhythm of the traditional shopfronts.
- The predominant two and three storey street wall height established by the heritage and contributory building facades should be matched in height by any new infill development.
- The 'Specific Policies and Recommendations' (Section 3.4) associated with the heritage items in this sub area (if applicable) should be taken into consideration as a part of the proposed development.

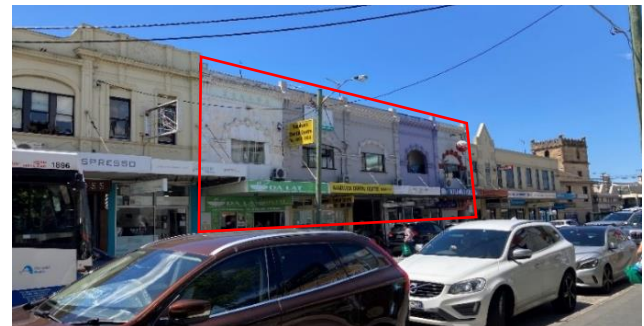
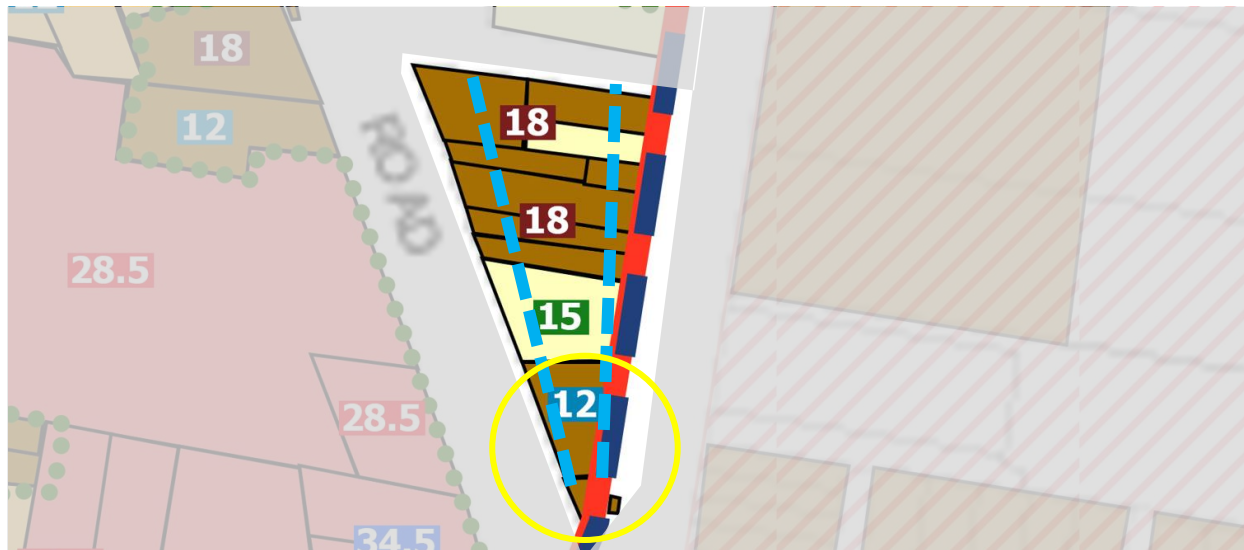
Sub Area 8 (Key Strategic Site)



Key Findings & Recommendations

- The subject sub area is one of the key strategic sites and currently comprises a contemporary shopping centre development with neutral characteristics. The proposed building height (28.5 metres) equivalent to seven storeys (2 shopping centre levels and 5 residential levels above) is acceptable from a heritage perspective.
- The proposed new development should have special design principles and characteristics, such as niche cultural type architecture similar to the next-door shops.
- A high quality contemporary architectural expression should be provided, rather than a superficial heritage pastiche.
- The design of a new development should strive for design excellence, innovation and creativity and somehow relate to each other and should be guided by a comprehensive heritage assessment in order to guide the proposed development.
- The existing building alignment should be maintained, and the podium level should match the existing parapet alignment along Belmore Road and not exceed 12m in height.
- The proposed development should avoid bulky and continuous block development.
- The 'Specific Policies and Recommendations' (Section 3.4) associated with the heritage items in this sub area (if applicable) should be taken into consideration as a part of the proposed development.

Sub Area 9



Source: Google Street View (November 2022).

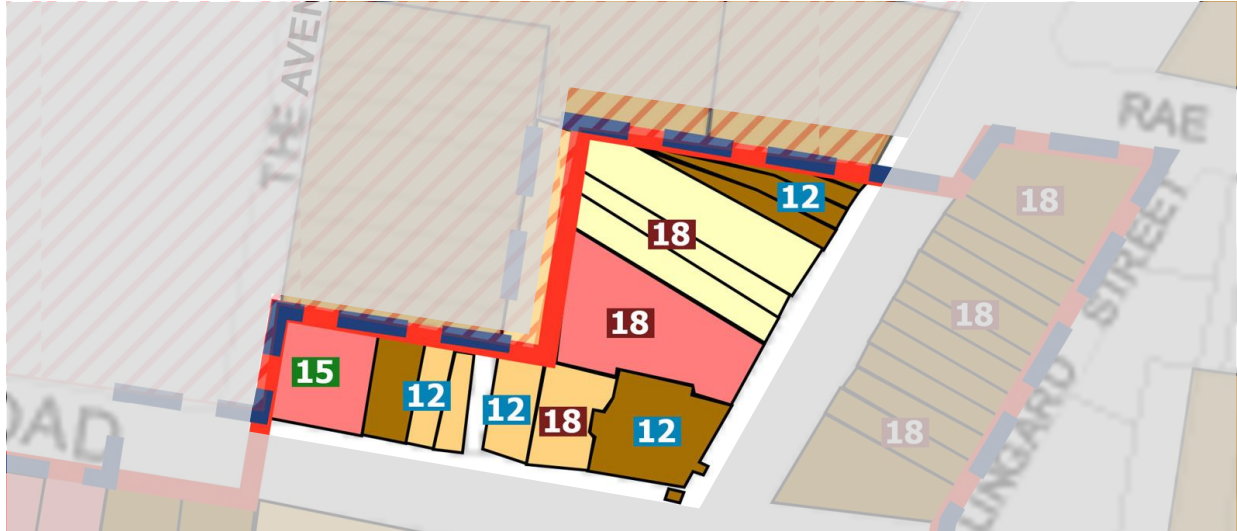
Key Findings & Recommendations

- No new development should be undertaken over the 'The Star and Garter Inn' (item no. I302) heritage item (refer to yellow circle on the plan). This heritage item can only accommodate an extension at the rear lower section on Belmore Road to match the height of the building as it is on Avoca Street façade. Any additional level even one level would impact on the free-standing prominent position and scale

of the tower. The new proposed 15 metres height starting from the neutral building at 135-139 Belmore Road will be appropriate behind the corner heritage item.

- Proposed new development, for the remainder of the triangular block should be setback at the upper level (behind the parapet of the street wall) from the existing building line, preferably to follow the alignment of Belmore Road, Avoca Street and Short Street (indicated in dash blue line based on the southeast and southwest parapet corners of the buildings at the intersections of Belmore Road, Avoca Street with Short Street to maintain the roofscapes of heritage items at 1 and 3 Short Street as shown on the aerial mark-up). Importantly, the castellated top of the tower must continue to be seen against the sky from prominent street eye level vantage points (surrounding footpath).
- Any new development should be setback (minimum 4m) from the building front i.e., the existing parapets. The existing, original parapet detailing should be retained.
- Any new development above the existing retail, double-storey buildings should have similar shopfront patterns and detailing on their facades and should be complementary to the existing significant characteristics.
- Encourage reinstatement of original façade detailing such as balconies/windows, where applicable and remove later additions from the facades, including metal framed, rectangular windows (indicated in red). This will assist in enhancing the heritage significance within Randwick Junction.
- Avoid continuous block development to retain narrow subdivision pattern and rhythm of the traditional shopfronts & develop a signage strategy for Belmore Road and other significant streetscapes within the town centre.
- The 'Specific Policies and Recommendations' (Section 3.4) associated with the heritage items in this sub area (if applicable) should be taken into consideration as a part of the proposed development.

Sub Area 10



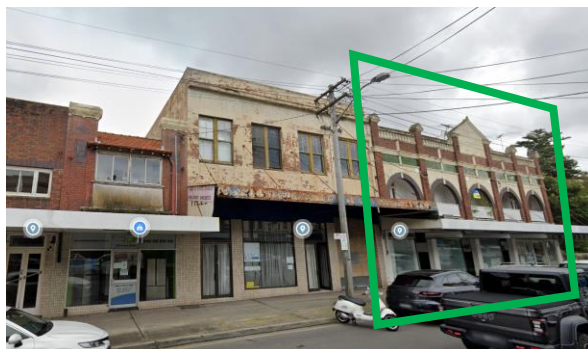
Source: Google Street View (November 2022).



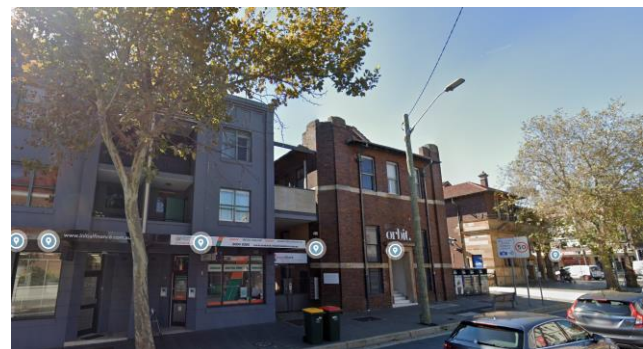
Source: Google Street View (November 2022).



Source: Google Street View (November 2022).



Source: Google Street View (November 2022).

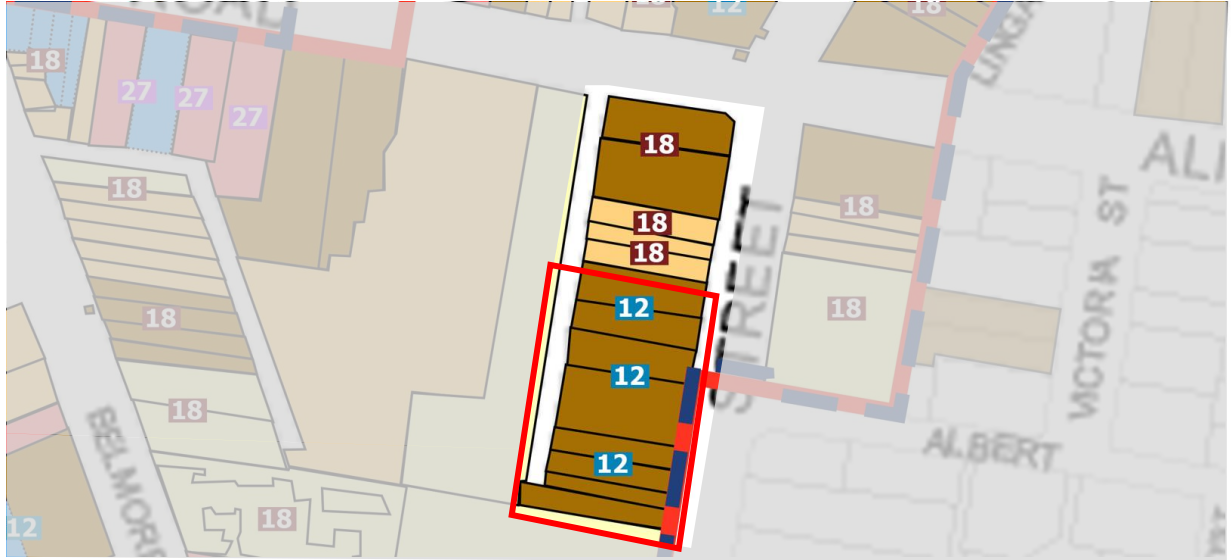


Source: Google Street View (November 2022).

Key Findings & Recommendations

- No new development should be undertaken over the three heritage items, including the state listed 'Randwick Post Office (former) and Jubilee Fountain' (SHR no. 01409).
- Any development should be setback at the upper level from both Avoca Street and Alison Road Street frontages, considering the impact on low-to-medium density development including heritage and contributing items. Any new development should be setback a minimum of 4m from the building front with existing parapets intact. The existing, original parapet detailing should be retained.
- Any new development above the existing retail, two and three storey buildings should have similar shopfront patterns and detailing on their facades and should be complementary to the existing significant characteristics.
- Encourage reinstatement of original façade detailing such as balconies/windows, where applicable and remove later additions from the facades, including metal framed, rectangular windows (indicated in green). This will assist in enhancing the heritage significance within Randwick Junction Town Centre.
- Develop a signage strategy for Avoca Street and other significant streetscapes within the town centre.
- 110-116 Avoca Street – The item is proposed to remain as the current 12m high development zone. The subject property has significant characteristics and detailing, including decorative front parapets, balustrades, pediments, facebrick arched balconies, and timber windows with coloured and clear glazing (indicated in green). The significant characteristics should be retained as a part of any the future development, which should be well setback from the building line with retention of original detailing. Later additions of intrusive detailing should be removed.
- If the detracting item located west of Avoca Street and north of Alison Road (telephone exchange) is proposed for redevelopment, the new development should have at least neutral characteristics and detailing and should not detract from the street presentation.
- The 'Specific Policies and Recommendations' (Section 3.4) associated with the heritage items in this sub area (if applicable) should be taken into consideration as a part of the proposed development.

Sub Area 11



Source: Google Street View (November 2022).



Source: Google Street View (November 2022).



Source: Google Street View (February 2023).



Source: Google Street View (November 2022).



Source: Google Street View (November 2022).

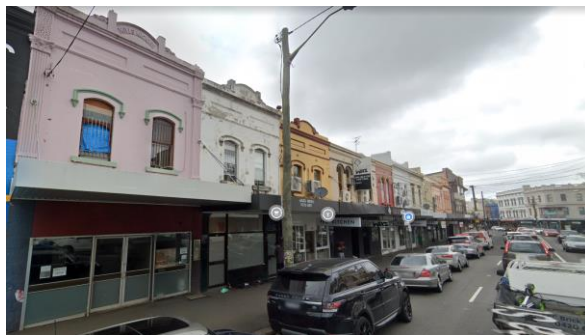
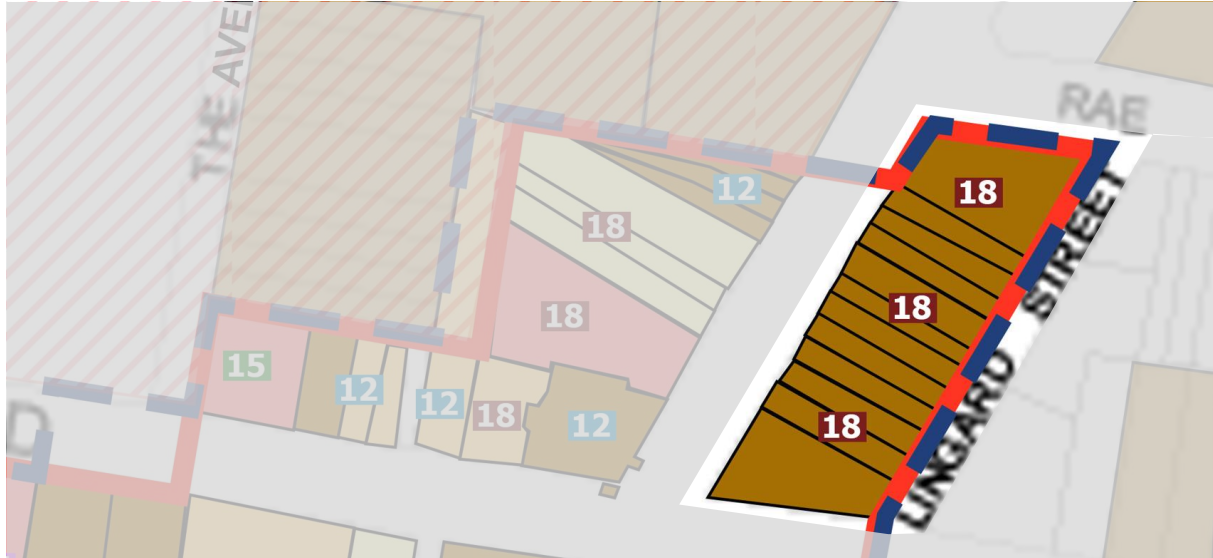
Key Findings & Recommendations

- The heritage item located at the corner of Avoca Street and Alison Road is proposed to accommodate a new height limit of 18 meters. This is acceptable from a heritage perspective.

However, any development should be well setback from both Avoca Street and Alison Road (minimum 4m) with existing parapets intact. The existing, original parapet detailing with horizontal decoration of cement balustrades should be retained.

- The building allotments along Avoca Street are comprised of heritage and contributory items that are proposed for the height upgrade to 18 meters. The proposed new development should have similar patterns and detailing on their facades and should be complementary to the existing significant characteristics. Continuous block development should be avoided at this portion of the sub area.
- No changes are proposed to the heritage items at 142-162 Avoca Street. Notwithstanding, any future development under the current height controls should ensure the overall characteristics, architectural integrity and scale of the Victorian and Federation houses with Italianate, gothic and filigree characteristics (indicated in red) are respected and retained in their current form.
- Encourage reinstatement of original façade detailing such as balconies/windows, where applicable and remove later additions from the facades, including metal framed, rectangular windows (indicated in red). This will assist in enhancing the heritage significance within Randwick Junction Town Centre.
- Develop a signage strategy for Avoca Street and other significant streetscapes within the town centre.
- The 'Specific Policies and Recommendations' (Section 3.4) associated with the heritage items in this sub area (if applicable) should be taken into consideration as a part of the proposed development.

Sub Area 12



Source: Google Street View (February 2023).



Source: Google Street View (November 2022).



Source: Google Street View (November 2022).



Source: Google Street View (February 2023).

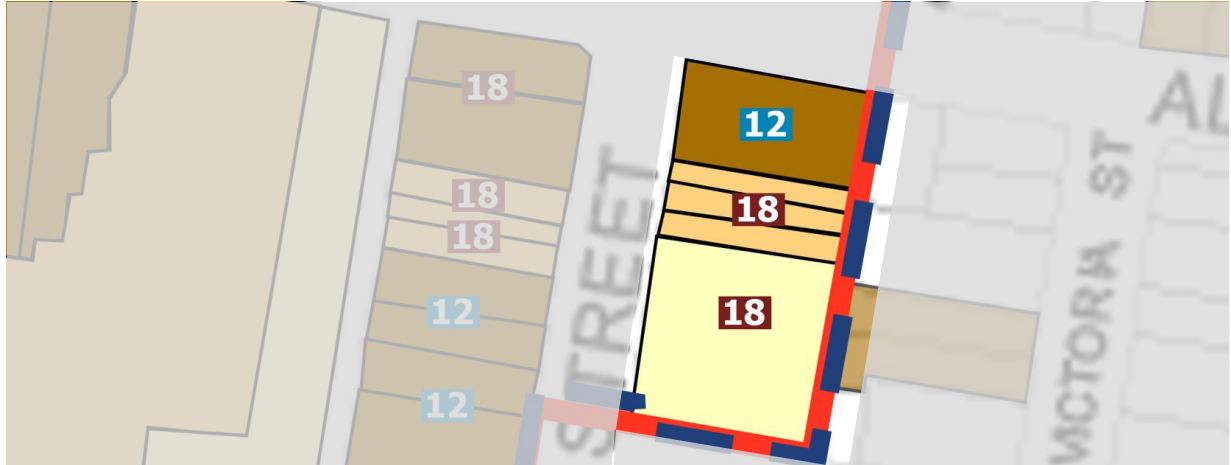


Source: Google Street View (November 2022).

Key Findings & Recommendations

- Avoid continuous block development along the east of Avoca Street. Retain a series of retail shops with well setback vertical additions, particularly along Avoca and Rae Streets and Alison Road, in broken and indented form to accommodate the proposed 18 metres height above the heritage items. The three-storey scale with high parapet of the heritage item at the corner of Alison Road and Avoca Street has the capacity to accommodate approximately 1.5 storey height bringing up the maximum stories to 5 stories under the proposed 18 metres height. This block should be developed with consideration of design excellence principles even if they are developed separately to ensure compatible architectural composition and consistency across the whole block.
- As noted above, the development should be setback at the upper level from Avoca Street, Alison Road and Rae Street, considering the potential impact on low density development including heritage and contributing items. The proposed new development should be well setback (minimum 4m) from the building front with existing parapets remaining intact. The existing, original parapet detailing should be retained.
- The proposed new development above the existing retail, two and three storey buildings should have similar shopfront patterns and detailing on their facades and should be complementary to the existing significant characteristics.
- Encourage reinstatement of original façade detailing such as windows, where applicable and remove later additions from the facades, including metal framed, rectangular windows (indicated in red). This will assist in enhancing the heritage significance within Randwick Junction Town Centre.
- Develop a signage strategy for Avoca Street and other significant streetscapes within the town centre.
- Any new development should maintain the existing prominent two and three storey street wall height, along the east of Avoca Street.
- The 'Specific Policies and Recommendations' (Section 3.4) associated with the heritage items in this sub area (if applicable) should be taken into consideration as a part of the proposed development.

Sub Area 13



Source: Google Street View (November 2022).

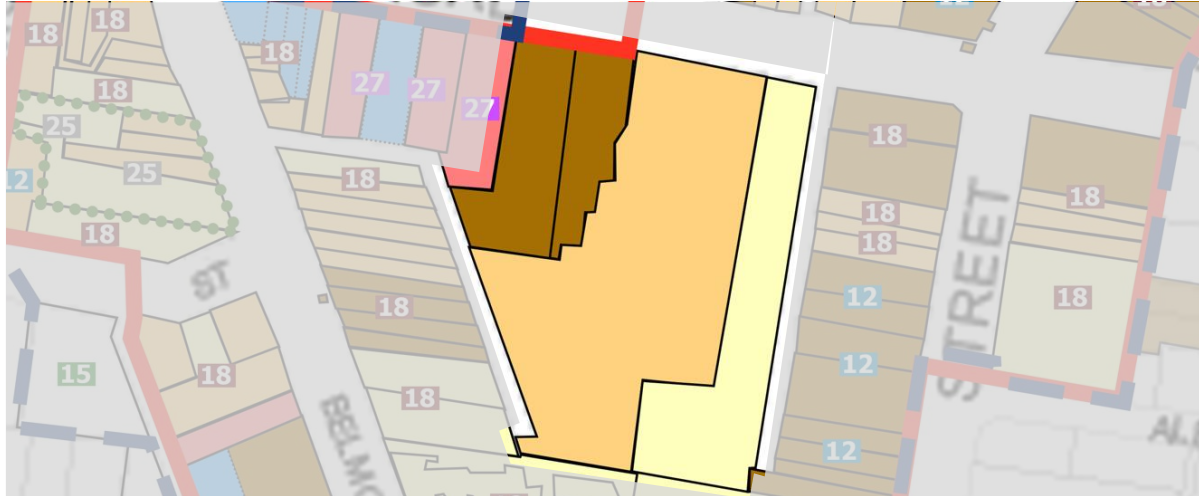


Source: Google Street View (November 2022).

Key Findings & Recommendations

- The heritage item located at the corner of Avoca Street and Alison Road (the Coach and Horses Hotel), is proposed to remain at the current 12m height limit, due to its high heritage value. Contributory items to the south are proposed to accommodate an increased permitted height to 18 meters. This is acceptable from a heritage perspective. The development should be well setback from both Avoca Street and Alison Road (minimum 4m) with existing parapets intact. The existing, original parapet detailing with horizontal moulding/detailing should be retained.
- Any new development should have similar patterns and detailing on their facades and should be complementary to the existing significant characteristics of heritage and contributory items. Continuous block development should be avoided at this portion of the sub area.
- Develop a signage strategy for Avoca Street and other significant streetscapes within the town centre.
- The 'Specific Policies and Recommendations' (Section 3.4) associated with the heritage items in this sub area (if applicable) should be taken into consideration as a part of the proposed development.

Marcellin College Campus (not part of Planning Proposal)



Source: Google Street View (November 2022).



Source: Google Street View (November 2022).

Key Findings & Recommendations

- This area, located south of Alison Road has two heritage items, one contributory item and one item of Neutral value. The subject sub area is not proposed for height upgrades and includes two outstanding Late Victorian houses which are the oldest and best survivors of the former Brisbane Estate illustrating the Boom Style architecture in Randwick.
- The two remaining Victorian houses are representative of Boom Style Architecture in Randwick and should not be developed in terms of height and bulk. The redevelopment of these early residences will not be feasible from a heritage perspective.
- No new development should be undertaken above or over the heritage item, it is important that the integrity of the sloped roofing (pitched or gable) is maintained.
- The contributory item, if developed should continue to retain characteristics that contribute to the Alison Road Streetscape and to the significant Randwick Junction Town Centre.
- The 'Specific Policies and Recommendations' (Section 3.4) associated with the heritage items in this sub area (if applicable) should be taken into consideration as a part of the proposed development.

8. CONCLUSION AND RECOMMENDATIONS

In conclusion, it is considered by City Plan Heritage that the proposed works of the Randwick Junction Town Centre Planning Strategy will have some, but acceptable impacts on the established and recognised unique heritage values of the Randwick Junction Town Centre, provided that the key findings and heritage recommendations of this report summarised below, are strictly implemented and reinforced by the Council through appropriate LEP and DCP controls.

As detailed in the Statement of Significance of Randwick Junction Town Centre, the area is a diverse and intact traditional commercial centre with many good examples of buildings from the Victorian, Federation and Inter-war period, which will need to be maintained and enhanced as part of the proposed planning strategy for the Town Centre incorporating site specific conservation approaches, a consistent signage strategy, streetscape revitalisation strategies while balancing the increased height and density in a compatible and appropriate manner. While the Key Findings and Recommendations for each Sub Area as detailed in Section 7 provide overarching advice on appropriate heights, setbacks and the potential for reinstatement and conservation of heritage and contributory buildings, there is a need for a more comprehensive streetscape study for the whole Town Centre or for individual sites as part of their future development to be required during the Pre-lodgement and Development Application (DA) consultation processes. The Key Findings and Recommendations identify for each site the requirements for additional and comprehensive studies, which are noted below.

The following heritage recommendations have been made to ensure adequate protection and careful management of the Randwick Junction Town Centre's heritage values:

- While maximum 4m podium setback along Belmore Road, Avoca Street and Alison Road should be retained for infill developments, podium heights should be based on the adjoining heritage item(s)' or contributory buildings' dominant parapet height to ensure the item(s)' dominance is maintained along the streetscape for continuation of the current people's experience at a human scale.
- Maintain a consistent upper level setback alignment behind the parapet along most of the Randwick Junction Town Centre study area to protect its unique heritage streetscape.
- All significant development should be guided by a comprehensive heritage assessment or a Conservation Management Strategy (now known as Heritage Asset Action Plans - HAAP) or Plan (CMS or CMP) in order to guide the management of established and assessed heritage significant items and contributory buildings.
- Conservation should be the paramount consideration. Heritage interpretation should also be an integral part of new development, ensuring the unique history, fine historic urban grain, narrow subdivision pattern, and the rhythm of the traditional urban shopping/commercial strip is maintained, respected and reflected in the future development.
- The aim of future development should be to celebrate, enhance and integrate historic built fabric, while at the same time highlighting the respective site(s) historical development.
- Where additional FSR is gained for a particular site through amalgamations and design excellence or other planning pathways, the preparation of a conservation management document becomes crucial in order to ensure the heritage aspects of the heritage item(s) and surrounding historic context is not detrimentally affected. Council should have strict rules on the preparation of such independent conservation studies and should endorse them, preferably at the preliminary stages of design proposals, feasibility studies and development applications.
- The design of new development should strive for design excellence, innovation and creativity and respond to the unique urban context.
- Podiums should be designed with consideration of the narrow subdivision pattern and rhythm of the traditional shopfronts.

- Repainting of Heritage items or contributory items should be in accordance with the appropriate heritage colour scheme to improve and enhance their contribution and remove uncharacteristic façade elements.
- The 'Specific Policies and Recommendations' associated with the heritage items (Section 3.4) and heritage conservation areas (Section 3.5) within the Randwick Junction Town Centre study area should be taken into consideration as a part of the proposed development.
- Randwick Junction Town Centre will significantly benefit from a signage strategy and possibly an overall heritage interpretation strategy framework that links all sites harmoniously increasing the experience of casual users and visitors in a narrative and visual appeal, adding to the entertainment and public art of the street and the surrounding streets (especially along Belmore Road and Avoca Street).

The following are provided as advisory recommendations for inclusion as conditions of consent in future development applications within Randwick Junction Town Centre study area to ensure new development is adequately managed in relation to heritage conservation.

Design Excellence

- Ensure high quality and varied design through the use of competitive design processes, such as an open or invited architectural design competition or competitive design alternatives for prominent developments, especially within key strategic sites and interfaces with heritage items.
- Ensure development individually and collectively contributes to the architectural and overall urban design quality of Randwick Junction Town Centre.
- Encourage variety in architectural design and character across large developments to provide a fine grain which enriches and enlivens Randwick City's public realm.

Restoration of Missing Details

- Consideration should be given to a commitment to the restoration of heritage items and contributory buildings forming part of a larger consolidated site redevelopment as a condition of DA consent. This should be included as part of a Schedule of Conservation Works recommended under a separate condition of consent.

Temporary Protection Plan

- Develop a temporary protection plan that identifies the potential risks and outlines measures to reduce the potential for damage to heritage fabric during the works, and adjoining heritage items and contributory buildings.

Conservation Management Plan or Strategy (CMP or CMS)

- A Conservation Management Plan or Strategy should be developed to ensure the future management and conservation of the respective heritage item. It should be prepared for the site prior to the undertaking of any physical works/ as a condition of consent/ to accompany another application. The conservation management plan should be prepared in accordance with the publication by the Heritage NSW, *Conservation Management Documents (2002)*.

Salvage

- Prior to commencement of the works, allow the built heritage specialist to identify significant internal elements for salvage and, if necessary, store, for reinstatement as appropriate. Removal of any items to be carried out in accordance with specific salvage methodologies provided by the built heritage specialist.

Monitoring

- A built heritage specialist is to be appointed for all major construction projects and during all critical processes that require specialist knowledge and methodology. Should any discoveries be made apparent during the absence of the built heritage specialist, they are to be notified immediately and work in that area is to cease.
- The built heritage specialist is to undertake regular inspections to suit the works. Timing and frequency to be agreed with the contractor.
- The built heritage specialist is to monitor the works and ensure that compliance conditions pertaining to heritage fabric are met.
- All new work associated with heritage fabric is to be documented in the site diary maintained by the built heritage specialist.
- All junctions between new and original fabric to be reversible and easily identifiable as such as defined in Article 15.2 of the Burra Charter.

Schedule of Conservation Works

- A built heritage specialist is to develop a schedule of conservation works that identifies the works required to remedy issues identified, as well guide repairs, restoration or reconstruction. The schedule of conservation works should be prepared in accordance with the Heritage NSW Maintenance Series.

Photographic Archival Recording

- A built heritage specialist is to develop an archival record (before, during and after) of areas impacted by the works in accordance with the Heritage NSW guidelines *Photographic recording of Heritage Items Using Film or Digital Capture (2006)*.

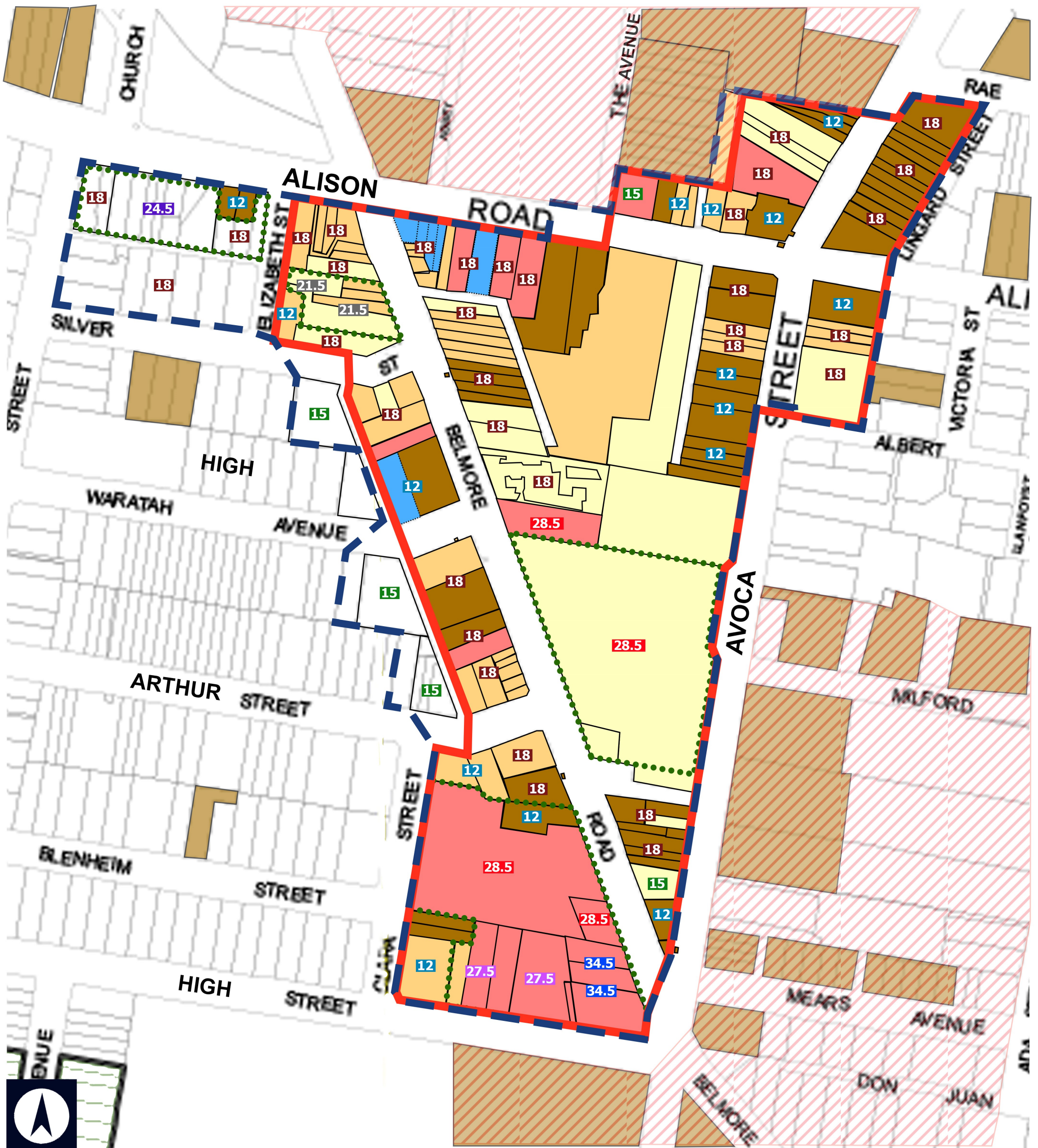
Heritage Interpretation

- A built heritage specialist is to develop a Heritage Interpretation Plan (HIP) for the proposed development in accordance with the Heritage NSW publications, *Interpreting Heritage Places and Items (2005)* and *Heritage Interpretation Policy (2005)*.

APPENDIX A

The Proposed Planning Strategy

Overlay of the Randwick Junction Town Centre Strategy over Heritage and Contributory Ranking Maps,
prepared by City Plan Heritage



LEGEND

	Randwick Junction HCA		Contributing Item		Proposed Heritage Item - General		12 metres		27.5 metres
	Randwick Junction Boundary		Neutral Item		Lots outside Randwick Junction HCA		15 metres		28.5 metres
	Heritage Item - General		Detracting Item		Key Strategic Site		18 metres		34.5 metres
	Heritage Item - General (outside the site boundary)		HCAs in the vicinity				21.5 metres		24.5 metres

RANDWICK JUNCTION TOWN CENTRE

June 2023

